

October 19, 2021

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATION
DEPARTMENT OF ADMINISTRATION

DIVISION OF PURCHASES BID NO. 7658841

RHODE ISLAND DEPARTMENT OF TRANSPORTATION

RHODE ISLAND CONTRACT NO.2021-CT-062

FEDERAL-AID PROJECT NO. FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Intersection Safety Improvements 2019 C2

Providence - Mount Pleasant Avenue between Smith Street (US Route 44) and Dover Street

Providence - North Main Street (US Route 1) at Doyle Avenue/Randall Street

Johnston - Hartford Avenue (US Route 6) at Bishop Hill Road

CITY/TOWN OF East Providence, Providence, Johnston, Barrington

COUNTY OF PROVIDENCE, BRISTOL

NOTICE TO PROSPECTIVE BIDDERS

ADDENDUM NO. 3 Prospective bidders and all concerned are hereby notified of the following changes in the Plans, Specifications, Proposal and Distribution of Quantities for this contract. These changes shall be incorporated in the Plans, Specifications, Proposal and Distribution of Quantities, and shall become an integral part of the Contract Documents.

A. Specifications - Job Specific

1. JS Index

Add new Index Page JS-iii, attached to this Addendum No. 3. A new Job Specific specification has been added.

2. JS-63 through JS-66

Add new Pages JS-63 through JS-66 attached to this Addendum No. 3. Special provision for Section 413, Rideability - Surface Course, has been added.

B. Distribution of Quantities

1. Index Pages

Delete Pages Index 1 through 6 in their entirety and replace them with revised Pages Index 1(R-1) through 6(R-1) attached to this Addendum No. 3. Items in bold have been revised.

2. Pages 16 through 18

Delete Pages 16 through 18 in their entirety and replace them with revised Page 16(R-1), new page 16a, revised Page 17(R-1), new page 17a, revised Page 18(R-1), and new Page 18a, attached to this Addendum No. 3. Item Code 401.1100, Modified Class 19.0 HMA, has been deleted. Item Code 401.3000, Class 9.5 HMA, has been revised. Item Code 401.3110, Modified Class 9.5 HMA, has been revised.

3. Page 66

Delete Page 66 in its entirety and replace it with revised Page 66(R-1), attached to this Addendum No. 3. Item Code 936.0110, Mobilization, has been deleted.

4. Page 116

Delete Page 116 in its entirety and replace it with revised Page 116(R-1), attached to this Addendum No. 3. Item Code 401.1000, Class 19.0 HMA, has been added. Item Code 936.0100, Mobilization and Demobilization, has been added.

C. Plans

1. Sheet No. 5 - Job Specific Legend & Notes

Delete Sheet No. 5 in its entirety and replace it with Revised Sheet No. 5(R-2), attached to this Addendum No. 3. The legend has been revised.

2. Sheet No. 9 - Construction Details 2

Delete Sheet No. 9 in its entirety and replace it with revised Sheet No. 9(R-2), attached to this Addendum No. 3. Details have been revised.

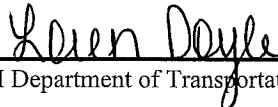
3. Sheet No. 12 - Mount Pleasant Ave - Typical Sections


Delete Sheet No. 12 in its entirety and replace it with revised Sheet No. 12(R-2), attached to this Addendum No. 3. The pavement legend and details have been revised.

D. Other Contract Changes

1. Federal Wage Rates

Delete all Federal Wage Rate pages and replace them with revised pages 1(R-1) through 13(R-1) attached to this Addendum No. 3. The Federal Wage Rates have been revised.

 10/21/21

RI Department of Transportation
 Administrator, Division of Project Management

INDEX
SPECIFICATIONS – JOB SPECIFIC

<u>CODE</u>	<u>TITLE</u>	<u>PAGE</u>
403.	Rideability – Surface Course	JS-63

SECTION 413
RIDEABILITY – SURFACE COURSE

413.01 DESCRIPTION. This specification covers pavement rideability as determined by the Engineer in accordance with the rating scale, based upon post-paving rideability determination.

413.02 MATERIALS. N/A

413.03 CONSTRUCTION METHODS. Pavement rideability, or ride quality, will be determined by the Engineer using a profiler on all travel lanes. A travel lane is defined as the primary traveled portion of the roadway excluding non-normally traveled pavement surfaces. The profiler will meet all the equipment requirements of AASHTO M 328 and R 56.

413.03.01 Rideability for Standard Roads. Standard roads shall be roadways with posted speed limits above 30 miles per hour (MPH). For roads with speed limits that vary based on time of day (e.g., school zones) the higher speed limit shall apply for the purposes of this specification.

The surface course ride quality acceptance will be based on the average International Roughness Index (IRI) from three tests using a profiler established for each wheel path for 528-foot (0.1-mile) standard fixed intervals, and a simulated 10-foot rolling straightedge analysis for the width of each travel lane from one representative test. The testing will be conducted by the Engineer. The selected tests will be chosen based on data correlation between tests, profile lengths, and any factors noted during data collection or evidenced upon review to better qualify or disqualify a test. The representative tests will be selected at the Engineer's discretion.

An IRI number in inches per mile will be established using ProVAL or software supplied by the manufacturer of the profiler for each 528-foot (0.1-mile) longitudinal section for each wheel path in each travel lane. A 300-foot long-wavelength (high-pass) filter will be applied during testing. A 250mm short-wavelength (moving average) filter will be applied during analysis using ProVAL or other aforementioned profiling software.

A subplot will be each single wheelpath for each 0.1-mile section of each travel lane. Therefore, each 0.1-mile section of travel lane will consist of two sublots. A standard lot is defined as 20 consecutive sublots. If a road segment has less than 20 but more than 6 consecutive sublots for each wheelpath, a lot will be comprised of all the sublots from one wheelpath. If a road segment has 6 or less consecutive sublots for each wheelpath, a lot will be comprised of all the sublots from the road segment. If the final lots include 10 or more sublots for each wheelpath, they will be considered their own lots. If the final lots are less than 10 sublots, they will be added to the preceding or adjacent lots. Lots may be combined with adjacent lots in the following order of preference: with previous lot of the same wheelpath in the same lane, with opposite wheelpath of the same lane, with both wheelpaths of adjacent lane in the same direction or similar lane in the opposite direction within the same road segment, or as determined by the Engineer. If project paving limits for one roadway segment including all lanes consist of less than 0.46 lane-miles, the roadway segment paved shall be considered a pavement patch (413.03.02).

Areas that are excluded from rideability analysis for pay adjustment ("leave-out" sections) include roundabouts and bridge decks, the area 25 feet before and after pavement segments with catch basins in the travel lane and 15 feet before and after pavement segments with manholes or other structures in the travel lane, the areas 25 feet before and after bridge joints, and 25 feet after and before project paving limits, all as determined by the Engineer. Additional areas may be excluded from testing in the interest

of obtaining data safely. All areas included in the paving limits (including “leave-out” sections) will be tested using a 10-foot rolling straightedge simulation analysis using ProVAL or other profiling software. The variation of the surface between any two contacts along the simulated straightedge shall be not more than 0.25 inches, except for manholes, catch basins and other structures in the travel lane which shall not deviate from the surface by more than 0.30 inches below final grade or 0.10 inches above final grade of pavement as tested with 10-foot straightedge or rolling straightedge simulation. Humps, depressions and utility structures (as adjusted or remaining) exceeding the specified tolerances shall be subject to correction at no additional cost to the State. The Contractor’s corrective work plan including method of correction shall be submitted to the Engineer for approval. No corrective work shall be performed without the approval of the Engineer.

Sections before “leave-out” sections and the section at the end of the paving limit will be added to the previous subplot or subsequent section(s) if they are less than 0.05 miles or may be considered a full subplot if they are greater than or equal to 0.05 miles. For roadways having many utility structures, with successive sections before or between “leave-out” sections or at the end of the paving limit that are less than or near to 0.05 miles, sections will be combined to best approximate 0.10 miles for each subplot. No subplot will exceed 0.15 miles.

413.03.02 Rideability for Low-Speed Roads and Pavement Patches. Low-speed roads shall be roads with a posted speed limit of 30 MPH or below. The Department will measure from 10 feet before to 10 feet after paving limits of low-speed roads or permanent pavement patches in the lane and test this road segment using a 10-foot rolling straightedge simulation analysis in ProVAL or other profiling software. If the variation of the surface between any two contacts along the simulated straightedge exceeds 0.25 inches at any distance from 2 feet before to 2 feet after the paving limits or pavement patch, corrective action shall be required and the Contractor must submit a corrective work plan. No corrective work shall be performed without the approval of the Engineer. Corrective work shall be at no cost to the State. In addition, the -0.30” to +0.10” surface deviation tolerances noted in Section 413.03.01 above shall apply for any manholes or other structures in the lane that are within the paving limits of the low-speed road or pavement patch.

Permanent pavement patches and structures, including any corrective work, must continue to comply with the tolerances described above for the longer of one year after installation or one year after completion of any required corrective work or additional corrective work will be required at no additional cost to the state.

413.03.03 Rideability for Bridges. The Department will measure the entire length of the bridge encounter using a 10-foot rolling straightedge simulation analysis in ProVAL or other profiling software. The bridge encounter starts 10 feet before the initial bridge pavement cut and match or 40 feet before the initial bridge joint, whichever is farther, and stops 10 feet beyond the terminal bridge pavement cut and match or 40 feet beyond the terminal bridge joint, whichever is farther. The bridge encounter includes approach pavement, entry approach slab, bridge deck, exit approach slab, and exit pavement. In the event only the bridge deck is paved (no approaches), the section of new pavement will be considered a pavement patch.

Perform corrective work when the variation of the surface between any two contacts along the simulated straightedge exceeds 0.25 inches in any profile within the full width of a marked traffic lane along the measured length of the bridge encounter. Submit a corrective work plan to the Engineer for approval prior to performing corrective work. This plan may include mill and overlay, PCC overlay, diamond grinding or combination thereof. Any costs associated with structural review of added loads to the bridge will be tracked by the Department and reimbursed by the contractor as a disincentive using a Report of Change.

In order to produce a uniform cross section, the Engineer may require corrections to the adjoining lanes and shoulders. No corrective work shall be performed without the approval of the Engineer. Corrective work shall be at no additional cost to the State.

413.04 METHOD OF MEASUREMENT

Table 1 provides the pay adjustment and corrective action criteria for standard roads based upon the posted speed limit of the roadway and the IRI of each subplot. IRI values shall be rounded to the nearest integer value for each subplot before assessing the pay adjustment.

Posted Speed Limit (MPH)				Pay Adjustment (Percent)
35	40-45	50	55-65	
IRI (Inches Per Mile)				
55 and Under	45 and Under	40 and Under	35 and Under	+5%
56 - 62	46 - 52	41 - 48	36 - 40	+4%
63 - 70	53 - 62	49 - 55	41 - 48	+3%
71 - 80	63 - 70	56 - 65	49 - 55	+1%
81 - 90	71 - 85	66 - 75	56 - 65	0%
91 - 110	86 - 95	76 - 82	66 - 72	-5%
111 - 125	96 - 110	83 - 92	73 - 82	-10%
126 - 145	111 - 125	93 - 105	83 - 95	-20%
146 - 160	126 - 140	106 - 120	96 - 110	-30%
Over 160	Over 140	Over 120	Over 110	Corrective Action Required*

***Corrective action will be required additionally for any section or segment with surface deviation(s) exceeding 0.25 inches when tested with a 10-foot straightedge or 10-foot rolling straightedge simulation analysis.**

When corrections to the pavement surface are required, the Contractor’s corrective work plan including method of correction shall be submitted to the Engineer for approval. The method of correction shall be limited to diamond grinding, flat tooth grinding or removing and replacing the affected pavement. If grinding is chosen, a grinding simulation using ProVAL shall be submitted to the Engineer for approval. In order to produce a uniform cross section, the Engineer may require corrections to the adjoining lanes and shoulders. Corrections shall be at no cost to the State. No corrective work shall be performed without the approval of the Engineer.

Where corrections are made after the official Department test, the pavement will be retested by the Engineer to verify that corrections have produced an acceptable ride surface. No incentives will be provided for sections on which corrective actions are performed.

In the event the corrective action(s) results in an IRI greater than the corrective action threshold for any subplot in any wheel path of a standard road, the Contractor will be assessed an adjustment based on **Table 2**.

Table 2				
Posted Speed Limit (MPH)				Pay Adjustment (Percent)
35	40-45	50	55-65	
IRI After Correction (Inches Per Mile)				
161 - 180	141 - 155	121 - 135	111 - 125	-50%
181 - 200	156 - 175	136 - 150	126 - 140	-75%
Over 200	Over 175	Over 150	Over 140	-100%

The pay adjustment for each subplot will be applied to the theoretical tonnage of each respective subplot. The theoretical subplot tonnage will be obtained by taking the subplot length, multiplied by half of the width of the travel lane (11 feet will be used for lane width unless otherwise indicated), multiplied by the design thickness of the surface course, multiplied by the unit weight derived from 94% of the average of the theoretical maximum densities for dense graded mixes or 90% of the theoretical maximum density for Friction Course or PPEST mixes. In the absence of plant tests for theoretical maximum density of any HMA or PPEST, average bulk specific gravity of mat final cores may be used to determine density. Theoretical tonnage values shall be rounded to 2 decimal places.

This rideability specification does not relieve the Contractor from responsibility concerning workmanship in accordance with the Specifications and other Contract requirements.

413.05 BASIS OF PAYMENT

A pay adjustment will be determined for each subplot based upon the IRI of that subplot as described above. The theoretical tonnages of all the sublots in one lot (as defined in 413.03 paragraph 4) will be totaled to yield the theoretical lot tonnage. The subplot tonnage adjustments will be obtained by multiplying the theoretical tonnage of a subplot by that subplot’s pay adjustment. Tonnage adjustments shall not be rounded. The subplot tonnage adjustments will be totaled to determine the lot tonnage adjustment. The lot tonnage adjustment will be divided by the theoretical lot tonnage to obtain the unit price adjustment for that lot. Unit price adjustment will be rounded to six absolute decimal places for each lot (i.e. XX.XXXX%). This unit price adjustment will be multiplied by the unit price of the surface course HMA and applied to the theoretical tonnage of the lot for pay purposes.

If the Contract does not have an item number exclusively for the surface course HMA as it has been grouped with related work such as tack coat, base course HMA or micro-milling, or surface course HMA does not otherwise have an explicit price basis, and if the Contractor has not submitted an accepted exclusive unit price basis for the surface course HMA prior to beginning any resurfacing or paving activity, the Department may use the Weighted Average Unit Price (WAUP) for the respective surface course HMA for rideability pay adjustment purposes.

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
201.0301	CUTTING AND DISPOSING ISOLATED TREES AND STUMPS (4"- 24")	1
201.0322	CLEARING AND GRUBBING	1
201.0401	REMOVE AND DISPOSE GRANITE CURB	1
201.0402	REMOVE AND DISPOSE CONCRETE CURB	2
201.0403	REMOVE AND DISPOSE SIDEWALKS	3
201.0407	REMOVE AND DISPOSE PAVEMENT AND RIGID BASE	5
201.0409	REMOVE AND DISPOSE FLEXIBLE PAVEMENT	5
201.0410	REMOVE AND DISPOSE CATCH BASINS	6
201.0414	REMOVE AND DISPOSE PIPE - ALL SIZES	6
201.0415	REMOVE AND DISPOSE GUARDRAIL AND POST ALL TYPES	6
201.0423	REMOVE AND DISPOSE HANDHOLE	7
201.0428	REMOVE AND DISPOSE FRAME AND GRATE OR FRAME AND COVER	7
201.0450	REMOVE AND STOCKPILE ON SITE GRANITE CURB	7
201.0453	REMOVE AND STOCKPILE ON SITE FRAME GRATES AND COVERS	9
201.0610	REMOVE AND DISPOSE DIRECTIONAL, WARNING, REGULATORY, SERVICE, AND STREET SIGNS	9
201.0613	REMOVE AND STOCKPILE LIGHT STANDARDS	10
201.0616	REMOVE AND DISPOSE LIGHT STANDARD FOUNDATIONS	10
201.0617	REMOVE AND DISPOSE CONDUIT - ALL SIZES	10
201.9901	REMOVE AND DISPOSE DELINEATOR AND POST	10
201.9902	REMOVE AND STOCKPILE ON SITE PRIVATE SIGN - "DAIRY CREME"	11
201.9903	REMOVE AND STOCKPILE ON SITE PRIVATE SIGN INCLUDING BASE AND MAILBOX	11
201.9904	REMOVE AND RESET BIKE RACKS	11
201.9905	** ITEM DELETED **	11
201.9906	REMOVE AND DISPOSE FIRE HYDRANT AND APPURTENANCES - BISHOP HILL ROAD	12
202.0100	EARTH EXCAVATION	12
202.9901	LOAD AND HUAL DRY CONTAMINATED SOIL, TYPE 2	12
202.9903	CONTAMINATED SOIL EXCAVATION	13
204.0100	TRIMMING AND FINE GRADING	13
206.0301	COMPOST FILTER SOCK	13
206.0312	COMPOST FILTER SOCK 12 INCH DIAMETER	14
209.0200	SACK INSERT CATCH BASIN INLET PROTECTION	14
212.2100	MAINTENANCE AND CLEANING OF EROSION AND POLLUTION CONTROLS	15
213.0100	PLACEMENT OF MILLINGS BENEATH GUARDRAIL	15
302.0100	GRAVEL BORROW SUBBASE COURSE	15
401.1100	** ITEM DELETED **	16
401.2000	CLASS 12.5 HMA	16
401.3000	CLASS 9.5 HMA	17
401.3003	CLASS 9.5 HMA FOR PATCHING	18
401.3100	MODIFIED CLASS 9.5 HMA	18
403.0300	ASPHALT EMULSION TACK COAT	18
403.9901	RUBBERIZED JOINT SEALER	19
501.0103	PORTLAND CEMENT CONCRETE BASE	19
502.1000	FULL DEPTH CLEANING AND SEALING OF JOINTS AND CRACKS 2- 1/2" NOM. WIDTH OR LESS IN PCC PAVEMENT	19
503.1010	REPAIRING DETERIORATED/DAMAGED JOINTS 2 1/2" TO 8" NOM. WIDTH IN FINISHED PCC PAVEMENT	20
601.0300	CLASS A PORTLAND CEMENT CONCRETE	20
701.0612	REINFORCED CONCRETE PIPE M 170 CLASS V 12 INCH	21
701.4312	12 INCH SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE	22
701.5206	6 INCH DUCTILE IRON WATER PIPE CLASS 52, RESTRAINED JOINT	22

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
701.9901	12" X 6" TAPPING SLEEVE AND VALVE WITH GATE BOX	22
702.0511	FRAME AND COVER STANDARD 6.1.0	22
702.0513	FRAME AND GRATE STANDARD 6.3.1	22
702.0521	FRAME AND COVER STANDARD 6.2.0	23
702.0605	PRECAST CATCH BASIN 4' DIAMETER STANDARD 4.4.0	23
702.0630	PRECAST MANHOLE 4' DIAMETER STANDARD 4.2.0	23
702.0711	DROP INLET BRICK STANDARD 3.6.0	23
702.0712	PRECAST CONCRETE DROP INLET STANDARD 4.5.0	24
702.0729	PRECAST CONCRETE DROP INLET, LATERAL OUTLET STANDARD 4.5.1	24
702.0730	PRECAST CONCRETE DROP INLET, LONGITUDINAL OUTLET STANDARD 4.5.2	24
702.9901	SIDEWALK MANHOLE FRAME AND COVER PROVIDENCE STANDARD 6.1.1	25
702.9902	HEAVY DUTY MANHOLE FRAME AND COVER - 24 INCH PROVIDENCE STANDARD 6.2.1	25
702.9905	GRANITE INLET STONE - 24" OPENING PROVIDENCE STANDARD 7.3.6	25
702.9906	SPECIAL MANHOLE - 4 FOOT DOGHOUSE	26
702.9907	FURNISH AND INSTALL NEW FRAME AND GRATE PROV. STD 6.3.4P	26
702.9908	FURNISH AND INSTALL CLEANOUT FRAME AND COVER PROVIDENCE STANDARD	27
702.9909	GRANITE APRON STONE - 24" OPENING STD 7.3.7 P	27
702.9910	GRANITE INLET STONE, 5-FOOT, STANDARD 7.3.5P	28
702.9911	HIGH CAPACITY GRATE AND THREE FLANGE FRAME PROVIDENCE STANDARD 6.3.4	28
702.9912	RECTANGULAR FRAME AND GRATE PROVIDENCE STANDARD 6.3.6	28
704.0300	RECONSTRUCT CATCH BASIN/VERTICAL WALLS	29
704.0400	RECONSTRUCT MANHOLE/VERTICAL WALLS	29
707.0900	ADJUST MANHOLES TO GRADE	29
707.0950	ADJUST TELEPHONE MANHOLE TO GRADE	30
707.0955	ADJUST ELECTRICAL MANHOLE TO GRADE	30
707.1000	ADJUST SANITARY MANHOLE	30
707.1200	ADJUST CATCH BASIN TO MANHOLE	31
707.1900	ADJUST FRAME & COVER TO GRADE	32
707.2000	ADJUST FRAME AND GRATE TO GRADE	32
708.9040	CLEANING AND FLUSHING PIPE ALL SIZES	32
708.9041	CLEANING CATCH BASINS ALL TYPES AND SIZES	33
708.9042	CLEANING MANHOLES ALL TYPES AND SIZES	34
709.8103	FURNISH & INSTALL CEMENT CONCRETE CLASS B THRUST & ANCHOR BLOCKS CAST-IN-PLACE	35
712.0100	WATER GATE BOX	35
712.0200	GAS GATE BOX	35
713.8268	ADJUST CURB STOP BOX TO GRADE	36
713.8269	ADJUST WATER GATE BOXES TO GRADE	36
713.8300	ADJUST GAS GATE BOXES TO GRADE	37
714.8163	POST TYPE HYDRANT	38
714.8263	REMOVE AND RELOCATE POST TYPE HYDRANT	38
901.0101	GUARDRAIL STEEL BEAM SINGLE FACE EARTH AND ASPHALT	38
901.0191	GUARDRAIL STEEL BEAM ANCHORAGE TRAILING END SECTION STANDARD 34.3.4	38
901.0199	GUARDRAIL END TREATMENT, ENERGY ABSORBING TERMINAL	38
905.0110	PORTLAND CEMENT SIDEWALK MONOLITHIC STANDARD 43.1.0	38
905.0115	PORTLAND CEMENT CONCRETE DRIVEWAY STANDARD 43.5.0	41
905.0131	PORTLAND CEMENT CONCRETE MEDIAN 6'' MONOLITHIC	42

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
906.0112	GRANITE CURB PROVIDENCE STANDARD 7" STRAIGHT	43
906.0113	GRANITE CURB PROVIDENCE STANDARD 7" CIRCULAR	44
906.0210	CEMENT CONCRETE CURB PRECAST STRAIGHT STANDARD 7.1.0	46
906.0211	CEMENT CONCRETE CURB PRECAST CIRCULAR STANDARD 7.1.0	46
906.0221	6' PRECAST CONCRETE TRANSITION CURB STANDARD 7.1.2	46
906.0250	PRECAST CONCRETE WHEELCHAIR RAMP CURB STANDARDS 7.1.3, 43.3.0 AND 43.3.1	47
906.0562	CEMENT CONCRETE EDGING 24 INCH LOT CURB STANDARD 7.2.3	47
906.0700	REMOVE, HANDLE, HAUL TRIM RESET CURB EDGING, STRAIGHT, CIRCULAR ALL TYPES	47
906.9901	6'-0" GRANITE TRANSITION CURB PROVIDENCE STANDARD 7.3.2 - 7"	49
906.9903	GRANITE RAMP STONE PROVIDENCE STANDARD 7.3.9 - 7" STRAIGHT	50
906.9904	GRANITE RAMP STONE PROVIDENCE STANDARD 7.3.9 - 7" CURVED	51
906.9905	GRANITE 2'-0" RADIUS CURB RETURN PROVIDENCE STANDARD 7.3.4	51
906.9906	GRANITE WHEELCHAIR RAMP TRANSITION CURB PROVIDENCE STD	52
906.9907	FURNISH AND INSTALL GRANITE INLET OR APRON STONE 7" WIDTH	53
907.0100	WATER FOR DUST CONTROL	54
914.5010	FLAGPERSONS	54
914.5020	FLAGPERSONS - OVERTIME	54
915.0200	HIGHWAY BOUNDS REINFORCED CONCRETE STANDARD 14.1.0	54
915.0240	REMOVE AND RESET HIGHWAY BOUNDS (ALL TYPES)	55
919.0101	TEST PITS	55
922.0100	TEMPORARY CONSTRUCTION SIGNS STANDARD 29.1.0 AND 27.1.1	55
923.0105	DRUM BARRICADE STANDARD 26.2.0	56
923.0125	PLASTIC PIPE TYPE III BARRICADE STANDARD 26.3.1	56
923.0200	FLUORESCENT TRAFFIC CONES STANDARD 26.1.0	56
924.0113	ADVANCE WARNING ARROW PANEL	56
925.0112	PORTABLE CHANGEABLE MESSAGE SIGN	56
928.0500	SHADOW OR ADVANCE WARNING VEHICLE WITH IMPACT ATTENUATOR AND FLASHING ARROW BOARD	57
929.0110	FIELD OFFICE	57
931.0110	CLEANING AND SWEEPING PAVEMENT	57
932.0100	CUTTING AND MATCHING ASPHALT	57
932.0200	FULL-DEPTH SAWCUT OF BITUMINOUS PAVEMENT	59
932.0210	FULL DEPTH SAWCUT OF BITUMINOUS PAVEMENT AND RIGID BASE	62
932.0220	FULL DEPTH SAWCUT OF BITUMINOUS SIDEWALK/DRIVEWAY	63
932.0230	FULL DEPTH SAWCUT OF PORTLAND CEMENT CONCRETE SIDEWALK/DRIVEWAY	64
935.0400	REMOVING BITUMINOUS PAVEMENT BY MICRO MILLING	65
936.0110	** ITEM DELETED **	66
937.0200	MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION	66
942.0200	DETECTABLE WARNING PANEL STANDARD 48.1.0	66
945.9911	REMOVE AND SALVAGE TRAFFIC SIGNAL EQUIPMENT - SIGNAL NO. 522	70
945.9912	REMOVE AND SALVAGE TRAFFIC SIGNAL EQUIPMENT - MOUNT PLEASANT AVENUE/COLLEGE ROAD/LESLIE DRIVE	70
945.9913	REMOVE AND SALVAGE TRAFFIC SIGNAL EQUIPMENT - MOUNT PLEASANT AVENUE/CHALKSTONE AVENUE	70
945.9915	REMOVE AND SALVAGE TRAFFIC SIGNAL EQUIPMENT - SIGNAL NO. 824	71
945.9916	REMOVE AND SALVAGE TRAFFIC SIGNAL EQUIPMENT - HARTFORD AVENUE/BISHOP HILL ROAD	71

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
945.9917	REMOVE AND DISPOSE TRAFFIC SIGNAL EQUIPMENT - FLASHING SCHOOL ZONE SIGN ASSEMBLY	71
999.9901	MISCELLANEOUS WORK	71
L01.0102	LOAM BORROW 4 INCHES DEEP	71
L01.0104	PLANTABLE SOIL 4 INCHES DEEP	73
L02.0101	GENERAL HIGHWAY SEEDING (TYPE 1)	73
L02.0102	RESIDENTIAL SEEDING (TYPE 2)	73
L05.0506	JUTE MESH	73
L06.9901	ZELKOVA SERRATA, "GREEN VASE" - 2" CALIPER	73
L10.0101	MECHANICAL TREE AND SHRUB ROOT PRUNING	74
L11.0102	TREE PLANT PROTECTION DEVICE STANDARD 51.1.0	75
L11.0104	DRIP-LINE TREE PROTECTION DEVICE STANDARD 51.1.1	77
T04.5001	6 AWG SINGLE CONDUCTOR CABLE 600V INSULATION	77
T04.5302	14 AWG 2 CONDUCTOR TWISTED SHIELDED CABLE	78
T04.5303	14 AWG 3 CONDUCTOR CABLE	78
T04.5305	14 AWG 5 CONDUCTOR CABLE	81
T04.6910	'10' STRANDED COPPER CONDUCTOR 600V INSULATION	83
T04.9901	OPTICALDETECTOR CABLE	84
T05.0100	PRECAST TYPE A HANDHOLE STANDARD 18.2.0	84
T05.0400	BREAK INTO EXISTING HANDHOLE	84
T05.1030	ADJUST HANDHOLE TO GRADE	85
T06.1020	2 IN. RIGID STEEL CONDUIT - UNDERGROUND	85
T06.1030	3 IN. RIGID STEEL CONDUIT-UNDERGROUND	86
T06.2020	2 IN. RIGID STEEL CONDUIT-OVERHEAD	86
T06.5130	3 INCH SCHEDULE 40 POLYVINYL CHLORIDE PLASTIC CONDUIT - UNDERGROUND	87
T06.5330	3 INCH SCHEDULE 40 POLYVINYL CHLORIDE PLASTIC CONDUIT - UNDER EXISTING PAVEMENT	87
T06.5430	3 INCH SCHEDULE 80 POLYVINYL CHLORIDE PLASTIC CONDUIT - UNDER EXISTING PAVEMENT	88
T06.6020	2 INCH POLYVINYL CHLORIDE PLASTIC CONDUIT-OVERHEAD	89
T07.9901	FURNISH AND INSTALL 145-165 WATT LED LUMINAIRE	89
T08.0100	LIGHT STANDARD FOUNDATION WITH ANCHOR BOLTS STANDARD 18.1.0	89
T08.2031	ALUMINUM LIGHTING STD. 30 FT. W/ SINGLE DAVIT ARM EXTN. 10 FT. STANDARD 18.3.0	90
T11.2008	TRAFFIC SIGNAL STANDARD,8 FOOT,STD 19.4.0 ALUMINUM PEDESTAL POLE AND FOUNDATION	90
T11.2010	TRAFFIC SIGNAL STANDARD,10 FT, STD 19.4.0 ALUMINUM PEDESTAL POLE AND FOUNDATION	91
T11.9901	55 FOOT GALVANIZED STEEL MAST ARM TRAFFIC SIGNAL POST AND FOUNDATION STD. 19.2.0	91
T12.0018	ACTUATED CONTROLLER TS-2, TYPE 1 W/8 PHASE ASSEMBLY GROUND MOUNTED INCLUDING FOUNDATION AND CABINET STD. 19.1.0	92
T12.9150	METER SOCKET W/MANUAL BY-PASS	92
T12.9901	MODIFY EXISTING TRAFFIC SIGNAL CONTROLLER/CABINET	92
T13.1000	TRAFFIC DETECTORS-LOOP, STANDARD 19.6.0	92
T13.1004	TRAFFIC DETECTOR RELAY-LOOP 4 CHANNEL	94
T13.8210	ACCESSIBLE PEDESTRIAN DETECTOR - PUSHBUTTON WITH SIGN	94
T13.9901	OPTICAL PHASE SELECTOR AND CHASSIS, FOUR CHANNEL	96
T13.9902	OPTICAL DETECTOR - SINGLE CHANNEL, ONE-WAY	96
T13.9903	OPTICAL DETECTOR CONFIRMATION BEACON	96
T14.3413	1 WAY 3 SECTION SPAN MOUNTED SIGNAL HEAD 12 INCH	96
T14.3513	1 WAY 3 SECTION MAST ARM MOUNTED SIGNAL HEAD 12 INCH	96

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
T14.3613	1 WAY 3 SECTION BRACKET MOUNTED SIGNAL HEAD 12 INCH	96
T14.3713	1 WAY 3 SECTION PEDESTAL MOUNTED SIGNAL HEAD 12 INCH	97
T14.9901	1-WAY PEDESTAL MOUNTED L.E.D. PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER, 12-INCH	97
T14.9902	2-WAY PEDESTAL MOUNTED L.E.D. PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER, 12-INCH	98
T14.9903	1-WAY BRACKET MOUNTED L.E.D. PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER, 12-INCH	98
T15.0100	DIRECTIONAL REGULATORY AND WARNING SIGNS	99
T15.1100	STREET SIGN - OVERHEAD MOUNTED	106
T15.9901	FLASHING SCHOOL ZONE SIGN ASSEMBLY	106
T15.9902	RECTANGULAR RAPID FLASHING BEACON ASSEMBLY	106
T15.9903	IN-STREET PEDESTRIAN X-ING SIGN W/SPRING LOADED PORTABLE SIGN BASE	107
T15.9910	REMOVE AND RELOCATE SIGN	107
T20.0706	6 INCH WHITE WATERBORNE PAINT PAVEMENT MARKINGS	107
T20.0712	12 INCH WHITE WATERBORNE PAINT PAVEMENT MARKINGS	108
T20.0904	4 INCH YELLOW WATERBORNE PAINT PAVEMENT MARKINGS	108
T20.1401	WATERBORNE PAINT PAVEMENT MARKING SYMBOL - ARROW (STRAIGHT, LEFT, RIGHT OR COMBINED) STANDARD 20.1.0	108
T20.1405	WATERBORNE PAINT PAVEMENT MARKING SYMBOL - YIELD LINE TRIANGLE (ALL SIZES)	108
T20.1410	WATERBORNE PAINT PAVEMENT MARKING WORD ("ONLY", "STOP", "YIELD", "AHEAD", "XING", "SCHOOL", OR OTHER) STANDARD 20.1.0	109
T20.1416	** ITEM DELETED **	109
T20.2406	6 INCH WHITE FINAL EPOXY RESIN PAVEMENT MARKINGS	109
T20.2412	12 INCH WHITE FINAL EPOXY RESIN PAVEMENT MARKINGS	110
T20.2804	4 INCH YELLOW FINAL EPOXY RESIN PAVEMENT MARKINGS	111
T20.2806	6 INCH YELLOW FINAL EPOXY RESIN PAVEMENT MARKINGS	112
T20.2812	12 INCH YELLOW FINAL EPOXY RESIN PAVEMENT MARKINGS	112
T20.3401	FINAL EPOXY RESIN PAVEMENT MARKING SYMBOL - ARROW (STRAIGHT, LEFT, RIGHT OR COMBINED) STANDARD 20.1.0	112
T20.3403	FINAL EPOXY RESIN PAVEMENT MARKING SYMBOL - LANE REDUCTION ARROW (LEFT OR RIGHT)	113
T20.3405	FINAL EPOXY RESIN PAVEMENT MARKING SYMBOL - YIELD LINE TRIANGLE (ALL SIZES)	113
T20.3410	FINAL EPOXY RESIN PAVEMENT MARKING WORD ("ONLY", "STOP", "YIELD", "AHEAD", "XING", "SCHOOL", OR OTHER) STANDARD 20.1.0	113
T20.3416	** ITEM DELETED **	114
T20.3417	** ITEM DELETED **	114

Table of Contents - Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

ItemCode	Description	Page
T20.9905	EPOXY RESIN PAVEMENT MARKINGS SPEED TABLE ARROW	115
202.9911	LOAD AND HAUL CONTAMINATED SOILS - TYPE 2	115
202.9912	DISPOSAL OF CONTAMINATED SOILS -TYPE 2	115
401.3005	CLASS 9.5 HMA FOR MISCELLANEOUS WORK	115
401.1000	CLASS 19.0 HMA	116
936.0100	MOBILIZATION AND DEMOBILIZATION	116

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
034	302.0100	Cont.				
		ST./DOYLE AVE.				
		FROM CODE 905.0110		215.00	0021	01
		FROM CODE 905.0115		15.00	0021	01
		FROM CODE 905.0131		33.00	0021	01
		N. MAIN STREET MEDIAN		134.00	0021	01
		RANDALL ST ISLAND & INT.		24.00	0021	01
		PROJECT WIDE				
		AS DIRECTED BY THE ENGINEER		76.00	0021	01
		RTE. 6/BISHOP HILL RD.				
		FROM ITEM 401.2100		271.00	0021	01
		FROM ITEM 401.3000		23.00	0021	01
		FROM ITEM 905.0110, 905.0115		35.00	0021	01
		FROM ITEMS		24.00	0021	01
		906.210,906.0211,9				
		06.0221,906.0250				
Item 302.0100 Total:				1,900.00		
035	401.1100	MODIFIED CLASS 19.0 HMA	TON			
		RTE. 6/BISHOP HILL RD.				
		260+17 - 262+15 RT			0021	01
		262+15 - 264+70 RT			0021	01
		264+70 - 267+70 RT			0021	01
Item 401.1100 Total:					**DELETED**	
036	401.2000	CLASS 12.5 HMA	TON			
		MOUNT PLEASANT AVENUE				
		00+00 - 12+08			0021	01
		00+00 - 12+08		810.00	0021	01
		15+27 - 16+71			0021	01
		15+27 - 16+71		100.00	0021	01
		23+87 - 25+40			0021	01
		23+87 - 25+40		150.00	0021	01
		26+94 - 27+77			0021	01

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
036	401.2000	Cont.				
		26+94 - 27+77		55.00	0021	01
		28+73 - 30+79			0021	01
		28+73 - 30+79		120.00	0021	01
		31+99 - 32+46			0021	01
		31+99 - 32+46		25.00	0021	01
		35+00 - 37+58			0021	01
		35+00 - 37+58		155.00	0021	01

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
036	401.2000	Cont.			0021	01
		42+98 - 44+07				
		42+98 - 44+07		70.00	0021	01
		47+48 - 48+52			0021	01
		47+48 - 48+52		70.00	0021	01
		57+71 - 59+17			0021	01
		57+71 - 59+17		125.00	0021	01
		60+40 - 63+70 RT		47.00	0021	01
		63+55 - 67+30			0021	01
		63+55 - 67+30		195.00	0021	01
		FARM ST			0021	01
		FARM ST		10.00	0021	01
		FROM ITEM 935.0400 (CITY			0044	02
		PROVIDENCE)				
		FROM ITEM 935.0400 (CITY		2,160.00	0044	02
		PROVIDENCE)				
		LENNON ST			0021	01
		LENNON ST		10.00	0021	01
		LESLIE DR (20+00)			0021	01
		LESLIE DR (20+00)		30.00	0021	01
		MOUNT PLEASANT AVENUE (RAISED				
		INTERSECTION)				
		11+65			0021	01
		30+30			0021	01
		35+70			0021	01
		ROANOKE ST			0021	01
		MOUNT PLEASANT AVENUE (RAISED				
		INTERSECTION)				
		11+65		15.00	0021	01
		30+30		15.00	0021	01
		35+70		15.00	0021	01
		ROANOKE ST		60.00	0021	01
Item 401.2000 Total:				4,237.00		

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
037	401.3000	CLASS 9.5 HMA	TON			
		MOUNT PLEASANT AVENUE				
		60+40 - 63+70 RT			0021	01
		NORTH MAIN ST./RANDALL				
		ST./DOYLE AVE.				
		DOYLE AVE		66.00	0021	01
		HOWES ST		28.00	0021	01
		NORTH MAIN SOUTH & MEDIAN		840.00	0021	01
		CONNECTOR				
		RANDALL ST		136.00	0021	01
		RTE. 6/BISHOP HILL RD.				
		260+17 - 262+15 RT		35.00	0021	01
		260+17 - 267+70		426.00	0021	01
		261+00 RT		4.00	0021	01
		262+15 - 264+70 RT		81.00	0021	01
		262+25 RT		7.00	0021	01
		262+60 RT		2.00	0021	01

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
037	401.3000 Cont.	264+00 LT		3.00	0021	01
		264+60 LT		3.00	0021	01
		264+70 - 267+70 RT		54.00	0021	01
Item 401.3000 Total:				1,666.00		
038	401.3003	CLASS 9.5 HMA FOR PATCHING	TON			
		MOUNT PLEASANT AVENUE				
		FROM ITEM 701.0612		70.00	0021	01
Item 401.3003 Total:				70.00		
039	401.3100	MODIFIED CLASS 9.5 HMA	TON			
		MOUNT PLEASANT AVE				
		60+40 - 63+70 RT		47.00	0021	01
		NORTH MAIN ST./RANDALL				
		ST./DOYLE AVE.				
		DOYLE AVE		66.00	0021	01
		FROM ITEM 501.0103		30.00	0021	01
		HOWES ST		28.00	0021	01
		N. MAIN ST, SOUTH & MEDIAN		840.00	0021	01
		CONNECTIONS				
		RANDALL ST		136.00	0021	01
		RTE. 6/BISHOP HILL RD.				
		260+17 - 262+15 Rt		35.00	0021	01
		260+17 - 267+70 (BCP1)		428.00	0021	01
		262+15 - 264+70 Rt		81.00	0021	01
		264+70 - 267+70 Rt		54.00	0021	01
Item 401.3100 Total:				1,745.00		
040	403.0300	ASPHALT EMULSION TACK COAT	SY			
		MOUNT PLEASANT AVENUE				
		60+40 - 63+70 RT		367.00	0021	01
		FROM ITEM 935.0400		14,700.00	0021	01
		FROM ITEM 935.0400 (CITY		16,780.00	0044	02

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
040	403.0300	Cont. PROV.)				
		RAISED INTERSECTIONS		640.00	0021	01
		NORTH MAIN ST./RANDALL				

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
129	935.0400	Cont. PROV.)				
		12+08 - 15+27		1,420.00	0044	02
		16+71 - 23+87		3,185.00	0044	02
		25+20 - 26+94		685.00	0044	02
		27+77 - 28+73		430.00	0044	02
		30+79 - 31+99		465.00	0044	02
		32+46 - 35+00		1,160.00	0044	02
		37+58 - 42+98		2,410.00	0044	02
		44+07 - 47+48		1,520.00	0044	02
		48+52 - 57+71		4,130.00	0044	02
		59+17 - 63+55		1,375.00	0044	02
		NORTH MAIN ST./RANDALL ST./DOYLE AVE. FROM ITEM 401.2100		10,620.00	0021	01
		RTE. 6/BISHOP HILL RD. 260+17 - 267+70		3,935.00	0021	01
		BISHOP HILL		300.00	0021	01
Item 935.0400 Total:				46,335.00		
130	936.0110	MOBILIZATION	LS			
		PROJECTWIDE				
		AS DIRECTED BY THE ENGINEER			0021	01
Item 936.0110 Total:				**DELETED**		
131	937.0200	MAINTENANCE AND MOVEMENT TRAFFIC	LS			
		PROTECTION				
		PROJECTWIDE				
		AS DIRECTED BY THE ENGINEER		1.00	0021	01
Item 937.0200 Total:				1.00		
132	942.0200	DETECTABLE WARNING PANEL STANDARD	SF			
		48.1.0				
		MOUNT PLEASANT AVENUE				

Distribution of Quantities

Project Name - Intersection Safety Improvements 2019 C2

Estimate Name - Addendum No. 3

R.I. Contract No. - 2021-CT-062

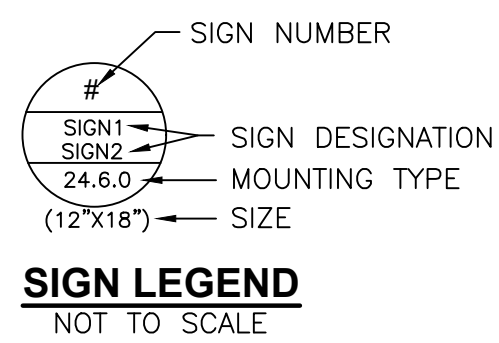
FAP Nos: 3RD-PRTY(312), STPG-HSIP(097)

Item No.	Item Code	Description	UM	Qty.	Pay Code	Seq. No.
214	401.3005	Cont.				
		262+25 RT		7.00	0021	01
		262+60 RT		2.00	0021	01
		264+00 LT		3.00	0021	01
		264+60 LT		3.00	0021	01
Item 401.3005 Total:				29.00		
215	401.1000	CLASS 19.0 HMA	TON			
		RTE. 6/BISHOP HILL RD.				
		260+17 - 262+15 RT		82.00	0021	01
		262+15 - 264+70 RT		195.00	0021	01
		264+70 - 267+70 RT		123.00	0021	01
Item 401.1000 Total:				400.00		
216	936.0100	MOBILIZATION AND DEMOBILIZATION	LS			
		PROJECT WIDE				
		AS DIRECTED BY THE ENGINEER		1.00	0021	01
Item 936.0100 Total:				1.00		

LEGEND

- (BCP1) MILL AND OVERLAY VARIABLE DEPTH (3"±) (LOCATIONS 2 & 3)
1.5" MODIFIED CLASS 9.5 HMA
1.5" CLASS 9.5 HMA
- (BCP2) MILL AND OVERLAY 2" (LOCATION 1)
2" CLASS 12.5 HMA
- (BCP3) RAISED INTERSECTION/CROSSWALK (LOCATION 1)
3" CLASS 12.5 HMA OVER
2" CLASS 12.5 HMA
- (BCP4) SHARED USE PATH (LOCATION 1)
2" MODIFIED CLASS 9.5 HMA OVER
2" CLASS 12.5 HMA OVER
8" GRAVEL BORROW
- (BCP5) FULL DEPTH WIDENING (LOCATION 2)
1.5" MODIFIED CLASS 9.5 HMA OVER
1.5" MODIFIED CLASS 9.5 HMA OVER
8" PORTLAND CEMENT CONCRETE BASE
- (BCP6) FULL DEPTH WIDENING (LOCATION 3)
1.5" MODIFIED CLASS 9.5 HMA OVER
3" CLASS 9.5 HMA OVER
8" CLASS 19.0 HMA BASE COURSE
12" GRAVEL BORROW
- (BCP7) DRIVEWAYS
3" CLASS 9.5 HMA OVER
8" GRAVEL BORROW
- (CCM6) PORTLAND CEMENT CONCRETE MEDIAN 6" MONOLITHIC
- (CFC) CLEANOUT FRAME AND COVER, PROVIDENCE STANDARD
- (CIDF) CURB INLET DRAIN FILTER
- (CPFL) CITY OF PROVIDENCE FUNDING LIMIT
- (DBM) REMOVE AND DISPOSE MAIL BOX
- (DDP) REMOVE AND DISPOSE DELINEATORS AND POSTS
- (DGC) REMOVE AND DISPOSE GRANITE CURB
- (DGMH) SPECIAL MANHOLE - 4 FOOT DOGHOUSE (SEE DETAIL)
- (DPS) REMOVE & DISPOSE PRIVATE SIGN
- (ETEAG) ENERGY ABSORBING TERMINAL END SECTION FOR GUARDRAIL
- (ETR) EXISTING TO REMAIN
- (MBC) MILLINGS BENEATH GUARDRAIL
- (ISCD) INLET SEDIMENT CONTROL DEVICE (SEE DETAIL)
- (MM) MICRO-MILLING (REFER TO GENERAL NOTE 7 THIS SHT.)
- (RCBC) RECONSTRUCT CATCH BASIN/CORBEL CONES
- (RCW) RAISED CROSSWALK (SEE DETAIL)
- (RDT) REMOVE & DISPOSE TREE
- (RFH) RELOCATE FIRE HYDRANT
- (SFC) REMOVE AND STACK FRAME AND COVER
- (SFG) REMOVE AND STACK FRAME AND GRATE
- (6.1.1P) SIDEWALK MANHOLE FRAME AND COVER PROVIDENCE STANDARD
- (6.2.1P) HEAVY DUTY MANHOLE FRAME AND COVER - 24 INCH PROVIDENCE STANDARD
- (6.3.4P) HIGH CAPACITY GRATE THREE FLANGE FRAME PROVIDENCE STANDARD
- (6.3.5P) HIGH CAPACITY FRAME AND GRATE PROVIDENCE STANDARD
- (6.3.6P) RECTANGULAR FRAME AND GRATE PROVIDENCE STANDARD
- (7.3.6P) GRANITE INLET STONE - 24 INCH OPENING PROVIDENCE STANDARD
- (7.3.0PS) GRANITE CURB PROVIDENCE STANDARD 7" STRAIGHT
- (7.3.0PC) GRANITE CURB PROVIDENCE STANDARD 7" CIRCULAR
- (7.3.2P) 6'-0" GRANITE TRANSITION CURB PROVIDENCE STANDARD 7"
- (7.3.3P) GRANITE CURB RAMP CURB PROVIDENCE STANDARD 7"
- (7.3.4P) GRANITE 2'-0" RADIUS CURB RETURN PROVIDENCE STANDARD
- (7.3.9) GRANITE RAMP STONE
- (7.3.9PS) GRANITE RAMP STONE PROVIDENCE STANDARD 7" STRAIGHT
- (7.3.9PC) GRANITE RAMP STONE PROVIDENCE STANDARD 7" CURVED
- (60.4.0P) INLET/APRON STONE REVEAL PROVIDENCE STANDARD

- (20.1.0) PAVEMENT MARKINGS - ARROWS AND ONLY
- (20.3.0) PAVEMENT MARKINGS - CROSSWALKS AND STOP LINES
- (20.4.0) PAVEMENT MARKINGS - YIELD LINE
- (20.4.0M) PAVEMENT MARKINGS - YIELD LINE (12"x18")
- (4.3.2) RAMP-LANDING FOR NARROW SIDEWALK
- (4.3.10P) CEMENT CONCRETE SIDEWALKS PROVIDENCE STANDARD
- (4.3.50P) CEMENT CONCRETE DRIVEWAYS PROVIDENCE STANDARD
- (BLM) BICYCLE LANE PAVEMENT MARKINGS
- (BSLM) BICYCLE SHARED LANE PAVEMENT MARKINGS
- (BWLL) 6" DASHED EPOXY RESIN PAVEMENT MARKINGS (SEE DETAIL)
- (CW) CROSSWALK - 2' WHITE EPOXY RESIN PAVEMENT MARKINGS WITH 2' GAP - PROVIDENCE STANDARD
- (DYSS) 4" DOUBLE YELLOW EPOXY RESIN PAVEMENT MARKINGS (2' STRIPE WITH 4' GAP)
- (4BY) 4" YELLOW EPOXY RESIN PAVEMENT MARKINGS (3' STRIPE WITH 9' GAP)
- (6WSS) 6" WHITE EPOXY RESIN PAVEMENT MARKINGS (2' STRIPE WITH 4' GAP)
- (6YSS) 6" YELLOW EPOXY RESIN PAVEMENT MARKINGS (2' STRIPE WITH 4' GAP)
- (LRAM) PAVEMENT MARKINGS - LANE-REDUCTION ARROW
- (RRFB) RECTANGULAR RAPID FLASHING BEACON - SOLAR POWERED (SEE DETAIL)
- (RRGB) REMOVE AND RELOCATE GRANITE BOUND
- (RSH) REMOVE EXISTING SIGNAL HEAD
- (SCPM) SCHOOL CROSSING AHEAD PAVEMENT MARKINGS
- (STPM) PAVEMENT MARKINGS - WORD "STOP"
- (#) WHEEL CHAIR RAMP NUMBER



GENERAL NOTES

- THE COST OF ANY CURB CUTTING REQUIRED TO INSTALL NEW OR RESET CURB WILL BE INCLUDED IN THE UNIT BID PRICE FOR THE NEW AND RESET CURB. SEE JOB SPECIFIC SPECIFICATION 906 " CUTTING GRANITE CURB".
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO WALLS, LOT CURBS, AND FENCES ABUTTING SIDEWALKS AND DRIVEWAYS DESIGNATED TO BE REMOVED. WHERE DIRECTED, NEW SIDEWALKS SHALL MEET SAID WALLS AND FENCES. PRIOR TO SIDEWALK REMOVAL, A SAWCUT SHALL BE PROVIDED IN ALL SIDEWALKS A DISTANCE DETERMINED BY THE ENGINEER (6" MINIMUM) FROM THE FACE OF ADJACENT BUILDINGS, RETAINING WALLS, LOT CURBS AND FENCES. THE FINAL 6" (MINIMUM) OF SIDEWALK SHALL BE REMOVED WITH CAUTION UNDER THE RESIDENT ENGINEER'S SUPERVISION. THE CONTRACTOR SHALL EMPLOY WHATEVER MEANS NECESSARY, INCLUDING HANDWORK, TO AVOID DAMAGING BUILDINGS, LOT CURBS, AND FENCES. THERE WILL BE NO ADDITIONAL PAYMENT FOR LABOR AND EQUIPMENT NECESSARY TO MEET THIS "REMOVE WITH CAUTION" REQUIREMENT. SAWCUTTING IN FRONT OF EXISTING BUILDINGS, WALLS, LOT CURBS, AND FENCES WILL BE INCLUDED IN THE COST TO REMOVE THE ADJACENT SIDEWALK.
- THE FOLLOWING CRITERIA SHALL APPLY FOR QUANTIFYING CODE 201.0610 "REMOVE AND DISPOSE DIRECTIONAL, WARNING, REGULATORY SERVICE AND STREET SIGNS" FOR PAYMENT ON THIS PROJECT:
 - MULTIPLE SIGNS ON A SINGLE POST OR MOUNTING SHALL BE CONSIDERED AS A SINGLE SIGN STRUCTURE AND THE REMOVAL OF SAID STRUCTURE SHALL BE QUANTIFIED AS ONE EACH.
 - REMOVAL OF MULTIPLE SIGNS FROM A UTILITY POLE SHALL BE QUANTIFIED AS ONE EACH FOR EACH SEPARATE SIGN REMOVED FROM SAID UTILITY POLE.
- THERE ARE NO LOCATIONS IN THE PROJECT AREA AND WITHIN THE STATE RIGHT-OF-WAY TO STORE CONSTRUCTION MATERIAL AND EQUIPMENT; THEREFORE, IN ACCORDANCE WITH SECTION 12.106.06 OF THE STATE OF RHODE ISLAND PROCUREMENT REGULATIONS, THE CONTRACTOR SHALL MAKE ARRANGEMENTS TO STORE CONSTRUCTION MATERIAL AND EQUIPMENT ELSEWHERE AT NO ADDITIONAL COST TO THE STATE.
- ALL TREE TRIMMING WORK WILL BE PERFORMED BY OTHERS THROUGH THE STATEWIDE TREE TRIMMING CONTRACT. PLEASE COORDINATE WITH THE RESIDENT ENGINEER IN ORDER TO ARRANGE TREE TRIMMING DURING CONSTRUCTION.
- THE CONTRACTOR SHALL NOT PARK VEHICLES OR CONSTRUCTION EQUIPMENT BENEATH THE DRIP LINE OF ANY TREES.
- THE MICRO-MILLING OPERATION FOR LOCATION 2 WILL REQUIRE MILLING THE ENTIRE EXISTING BITUMINOUS PAVEMENT FROM THE EXISTING CONCRETE BASE COURSE.
- ~~SPEED HUMP PAVEMENT MARKINGS ON SPEED LUMPS SHALL BE INCIDENTAL TO THE SPEED LUMP PROVIDENCE STANDARD (SPL) ITEM.~~
- EXISTING STRAIGHT GRANITE CURB AND EXISTING GRANITE INLET STONE PROPOSED TO BE REMOVED AND SALVAGED ON LOCATION 1 AND LOCATION 2 (PROVIDENCE) SHALL BE DELIVERED TO THE PROVIDENCE DEPARTMENT OF PUBLIC WORKS (DPW) YARD AT 20 EARNEST STREET, PROVIDENCE, RHODE ISLAND 02905. STRAIGHT GRANITE CURB WITH BIG BELL FLARES ON THE BOTTOM WILL NOT BE ACCEPTED AND SHALL BE REMOVED AND DISPOSED. THE CONTRACTOR SHALL COORDINATE ANY SALVAGED ITEMS AS DESCRIBED WITH DAVID MAMBRO OF THE PROVIDENCE DPW AT (401) 919-0262.

CURB RAMP NOTES

- WHERE IT IS NECESSARY TO CUT EXISTING GRANITE CURB ADJACENT TO A NEW CURB RAMP INSTALLATION, THE CURB SECTION TO BE CUT WILL BE REMOVED, CUT AND RESET SO THAT NO RESET OR REMAINING CURB SECTION IS LESS THAN 3'-0" IN LENGTH. THE COST OF REMOVING, CUTTING AND RESETTING CURB WILL BE INCLUDED IN THE CONTRACT UNIT BID PRICE PER LINEAR FOOT FOR ITEM CODE 906.0700 "REMOVE, HANDLE, HAUL, TRIM, RESET CURB ENDING, STRAIGHT CIRCULAR ALL TYPES FOR THE LENGTH OF THE CURB TO BE RESET. THE SECTION OF EXISTING CURB TO BE REMOVED AND DISPOSED WILL BE PAID FOR UNDER ITEM CODE 201.0401 "REMOVE AND DISPOSE GRANITE CURB". CONCRETE CURB SECTIONS SHALL BE FULLY REPLACED, NO CUTS OR PARTIAL SECTIONS SHALL BE PERMITTED.
- CONCRETE SIDEWALKS ARE TO BE REPLACED TO THE NEXT JOINT, NO PARTIAL SAWCUT AREAS OF SIDEWALK SHALL BE PERMITTED TO REMAIN.
- THE FINAL LOCATION OF ALL WHEELCHAIR RAMPS SHALL BE COORDINATED IN THE FIELD WITH PROPOSED AND/OR EXISTING LOCATIONS OF DRAINAGE STRUCTURES, UTILITY POLES AND OTHER APPURTENANCES TO ENSURE A CLEAR PEDESTRIAN ACCESS ROUTE IN ACCORDANCE WITH THE UNITED STATES ACCESS BOARD, PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN PUBLIC RIGHTS-OF-WAY, 2011 (PROWAG).
- THE GRADES PROVIDED ARE FOR REFERENCE IN CONSTRUCTING ADA COMPLIANT CURB RAMPS. THE CONTRACTOR SHALL INSTALL THE WHEELCHAIR RAMPS IN ACCORDANCE WITH THE R.I. STD. DETAILS 43.3.0, 43.3.1 AND 43.3.2. SLOPE MAXIMUMS ON LEVEL LANDING AND TRANSITIONS TO EXISTING CONDITIONS SHALL NOT BE EXCEEDED.
- THE CONTRACTOR SHALL INSURE THAT NO LOW POINTS ARE CREATED WITHIN THE WHEELCHAIR RAMP PROPER AND THAT NO PONDING SHALL OCCUR ALONG THE GUTTER LINE OR WITHIN THE LEVEL LANDING AREA. PROPER ADJUSTMENTS SHALL BE MADE AT THE DIRECTION OF THE ENGINEER.
- ALL SLOPES SHOWN ON THE PLANS WERE PROVIDED BASED ON CONTOURS SHOWN ON THE PLANS. EXISTING CONTOURS WHERE INCLUDED WITH BASE MAPPING PROVIDED TO GORDON R. ARCHIBALD, INC. THE CONTRACTOR SHALL VERIFY ALL GRADES PRIOR TO CONSTRUCTION. ANY DEVIATION SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.

ENVIRONMENTAL PERMIT NOTES

- THE WORK PROPOSED IN THESE PLANS SHALL NOT RESULT IN ANY ROADWAY GRADE CHANGES.
- ALL WORK SHALL BE IN COMPLIANCE WITH RULE 6.01 "GENERAL CONDITIONS FOR EXEMPT ACTIVITIES" OF THE RIDEM, DIVISION OF FRESHWATER WETLANDS "RULES AND REGULATIONS GOVERNING THE ADMINISTRATION AND ENFORCEMENT OF THE FRESHWATER WETLANDS ACT".

TRAFFIC SIGNAL NOTES

- THE CONTRACTOR SHALL REPLACE THE EXISTING EQUIPMENT AS NOTED ON THE PLANS AND REWIRE TO PROVIDE A FULLY FUNCTIONAL SIGNAL CONSISTENT WITH EXISTING FUNCTIONALITY AND OPERATIONS.
- NEW LOOP DETECTOR TIE-INS SHALL UTILIZE EXISTING FLEX DUCT AND BE SPLICED INTO ADJACENT HANDHOLES UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER.
- AN INDIVIDUAL CONTROL UNIT FOR EACH ACCESSIBLE PEDESTRIAN DETECTOR SHALL BE INSTALLED IN THE ASSOCIATED PEDESTRIAN SIGNAL HEAD OR IN THE DETECTOR HOUSING ITSELF. A CENTRAL CONTROL UNIT IN THE TRAFFIC SIGNAL CONTROLLER CABINET FOR THE ACCESSIBLE PEDESTRIAN DETECTORS SHALL NOT BE FURNISHED OR INSTALLED.

BASE MAPPING AND SURVEY NOTES

- THESE PLANS WERE DEVELOPED ON BASE MAPPING AND SURVEY PROVIDED TO GORDON R. ARCHIBALD, INC. (GRA) BY RIDOT AS PREPARED BY BETA, INC., LINCOLN, RI. BASE MAPPING PROVIDED TO GRA INCLUDED ALL TOPOGRAPHIC MAPPING, BASELINE DATA AND LAYOUT, RIGHT-OF-WAY, AND UTILITIES (INCLUDING EXISTING INVERTS). GRA DOES NOT WARRANT THE ACCURACY OF THE BASE PLANS, CONTROL OR UTILITY LOCATIONS. THE CONTRACTOR SHALL FIELD VERIFY ALL LOCATIONS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS.
- EXISTING TOPOGRAPHY SHOWN ON THE PLANS IS BASED ON ACTUAL FIELD SURVEY PERFORMED BY INSITE ENGINEERING SERVICES, LLC.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES

- TRAFFIC FINES IN WORK ZONE REGULATORY SIGN, R.I. STD. 27.1.1 SHALL BE UTILIZED WHENEVER CONSTRUCTION ACTIVITIES ARE IN PROGRESS AND CONSTRUCTION PERSONNEL ARE PRESENT. THE SIGNS SHALL BE REMOVED FROM THE SITE OR COVERED AT ALL OTHER TIMES.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT ALL APPLICABLE REGULATORY AND WARNING SIGNS (EXCEPT FOR PARKING SIGNS) ARE VISIBLE TO TRAFFIC AT ALL TIMES ON EITHER PERMANENT OR TEMPORARY MOUNTINGS.
- THE INTENDED PEDESTRIAN ACCESS ROUTES THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES.

DRAINAGE AND UTILITY NOTES

- DURING THE PROCESS OF THE WORK, THE CONTRACTOR SHALL COOPERATE WITH THE OWNERS OF ADJACENT OVERHEAD OR UNDERGROUND UTILITIES AND PERMIT THEIR REPRESENTATIVES ACCESS TO THE WORK AREA TO DETERMINE IF THEIR UTILITIES ARE BEING ENDANGERED IN ANY WAY.
- ANY DRAINAGE STRUCTURES, PIPES OR CULVERTS DAMAGED BY THE CONTRACTOR WHILE CARRYING OUT THIS CONTRACT SHALL BE REPLACED OR REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL CHARGE TO THE STATE.
- THE PER EACH PAYMENT FOR CODE 707.1100 "ADJUST CATCH BASINS" SHALL INCLUDE FULL COMPENSATION FOR RESETTING BOTH FRAMES AND GRATES AND FRAMES AND COVERS AS REQUIRED, AND MEASURED FOR PAYMENT PER EACH CATCH BASIN SO TREATED.
- COMPENSATION FOR THE ADJUSTMENT OF NEW FRAMES AND GRATE AND FRAMES AND COVERS TO FINAL GRADE AS WELL AS ANY INTERMEDIATE ADJUSTMENTS SHALL BE INCLUDED IN THE CONTRACT BID PRICES FOR 702.0517 "FRAME AND GRATE, R.I. STD. 6.3.2" AND 702.0522 "FRAME AND COVER, R.I. STD. 6.2.1". PAYMENT FOR THESE ITEMS SHALL INCLUDE FULL COMPENSATION FOR ALL ITEMS INCLUDED IN ADJUSTING DRAINAGE STRUCTURES AS PER SUBSECTION 707.05 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL ADJUST ALL DRAINAGE AND UTILITY STRUCTURES IN ROADWAY AREAS TO THE SURROUNDING PAVEMENT DRAINAGE AT THE END OF EACH WORK DAY. THE CONTRACT PROVIDES FOR THE FOLLOWING ADJUSTMENTS: ONE PRIOR TO BITUMINOUS PAVEMENT REMOVAL BY MICRO-MILLING, ONE TO GRADE OF LEVELING COURSE (WHERE APPLICABLE), AND ONE TO FINAL GRADE.
- ANY/ALL ADJUSTMENTS TO VERIZON OWNED UNDERGROUND EQUIPMENT (MANHOLES, FRAMES AND COVERS, CONDUITS, ETC.) MUST BE PERFORMED BY AN APPROVED VERIZON CONTRACTOR. ADDITIONALLY, ALL ADJUSTMENTS MUST BE INSPECTED BY VERIZON'S CONTRACT WORK INSPECTOR (CWI).
- IF VERIZON'S UNDERGROUND STRUCTURES (MANHOLES, CONDUIT, CABLES, ETC.) ARE EXPOSED DURING CONSTRUCTION, THE GENERAL CONTRACTOR MUST PROVIDE PROTECTION FOR THE EXPOSED PLANT IN ACCORDANCE WITH VERIZON'S METHODS AND PROCEDURES AND WITH THE APPROVAL OF VERIZON'S CWI. ADDITIONALLY, AN APPROVED PARTITION MUST BE PLACED BETWEEN EXISTING VERIZON STRUCTURES AND NEW CONCRETE CONSTRUCTION WHERE CONTACT AND/OR ENCROACHMENT MAY ARISE.
- THE UNDERMINING OF VERIZON DUCTS (INCLUDING THOSE CONCRETE ENCASED) IS NOT PERMITTED WITHOUT INSPECTION/PERMISSION OF VERIZON'S CWI.
- AS A MINIMUM, CLEARANCE TO OVERHEAD POWER AND COMMUNICATIONS FACILITIES SHALL CONFORM TO THE NATIONAL ELECTRIC SAFETY CODE (NEC) AND OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REQUIREMENTS. PARTICULAR ATTENTION MUST BE PAID TO THE OSHA REQUIREMENT 29 CFR 1910.333, SUBPART C AND 29 CFR 1926.550 SUBPART N AS THESE REQUIREMENTS GOVERN IN MOST CASES WHEN DEALING WITH POWER LINES. THE SUBPARTS REQUIRE ALL PERSONS OF OBJECT/EQUIPMENT THAT A PERSON IS IN CONTACT WITH SHALL NOT COME CLOSER TO ANY UNGUARDED, ENERGIZED OVERHEAD (PRIMARY ELECTRICAL) LINE THAN THE FOLLOWING DISTANCES:
 - FOR VOLTAGES TO GROUND 50KV OR BELOW - 10 FEET.
 - FOR VOLTAGES TO GROUND OVER 50KV - 10 FEET, PLUS 4 INCHES FOR EVERY 10KV OVER 50KV.

ALSO, THE NEC STATES THAT VERTICAL AND HORIZONTAL CLEARANCES OF 3 FEET MUST BE MAINTAINED FOR INSULATED COMMUNICATION CONDUCTORS AND CABLES. A RADIAL CLEARANCE OF 3 FEET MUST BE MAINTAINED BETWEEN VERIZON'S AERIAL EQUIPMENT (CABLES, TERMINALS, POLES, ETC.) IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE AND THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATIONS REQUIREMENTS. THIS INCLUDES TRAFFIC SIGNAL AND CONSTRUCTION EQUIPMENT, EITHER TEMPORARY OR PERMANENT.
- ALL DRAINAGE PIPES WITHIN THE PROJECT LIMITS, NOT DIRECTLY CONNECTED TO THE COMBINED SEWER SYSTEM, SHALL BE CLEANED AND FLUSHED, AND BE PAID FOR UNDER ITEM 708.9040. ALL CATCH BASINS WITHIN THE PROJECT LIMITS SHALL BE CLEANED AND PAID FOR UNDER ITEM 708.9041.

ADDENDUM NO. 3

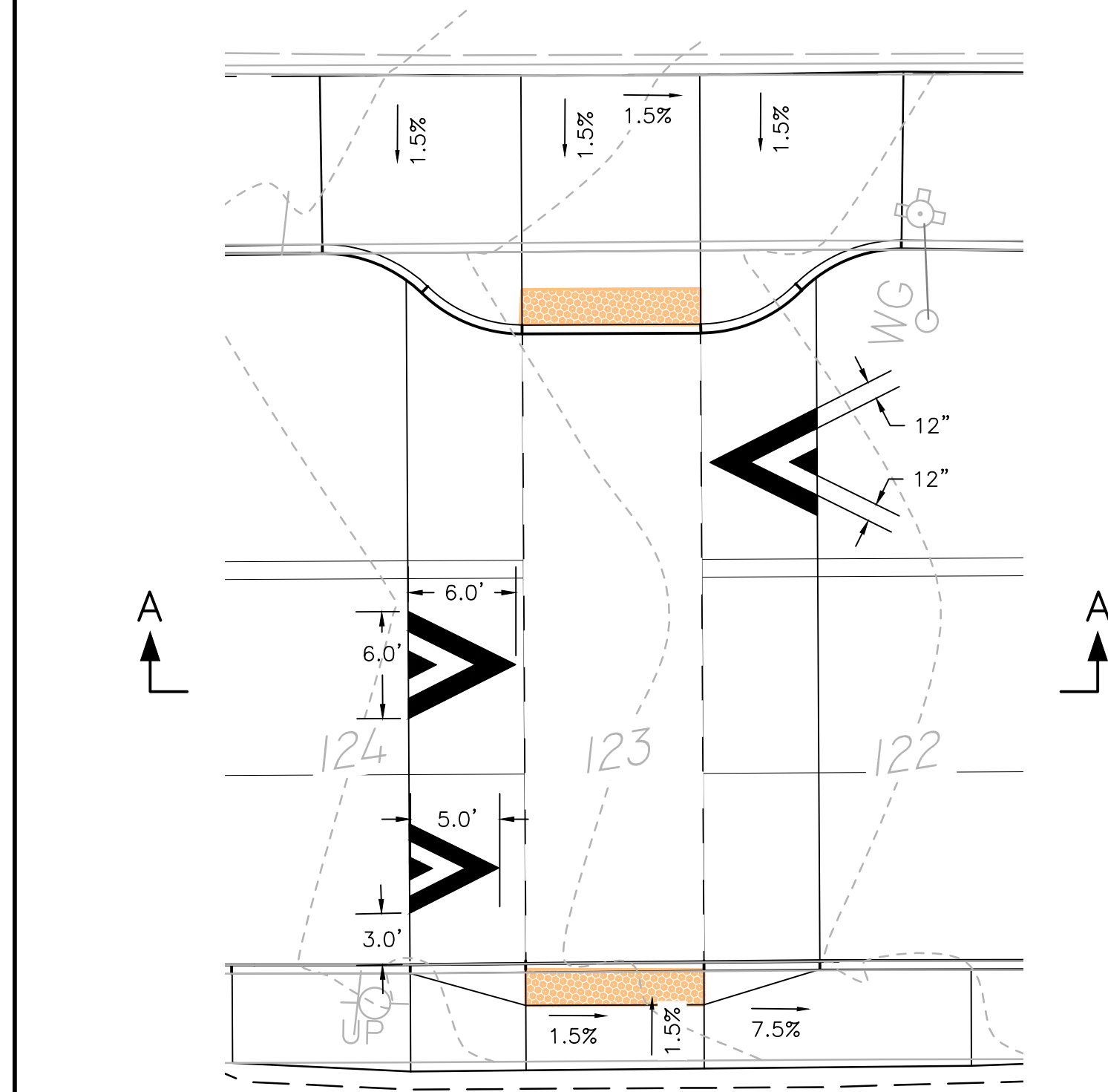


RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

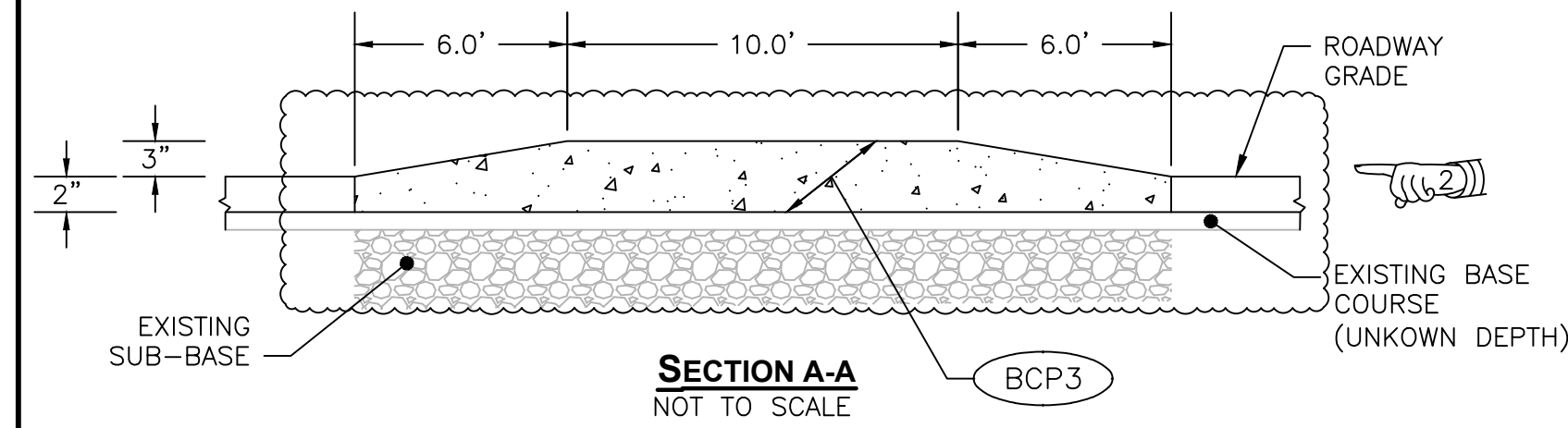
DESIGNED BY:
CHECKED BY:
DATE: SEPTEMBER 2021
SHEET: 5
OF: 77

NO SCALE	
NO.	DATE
1	10/6/21
2	10/16/21

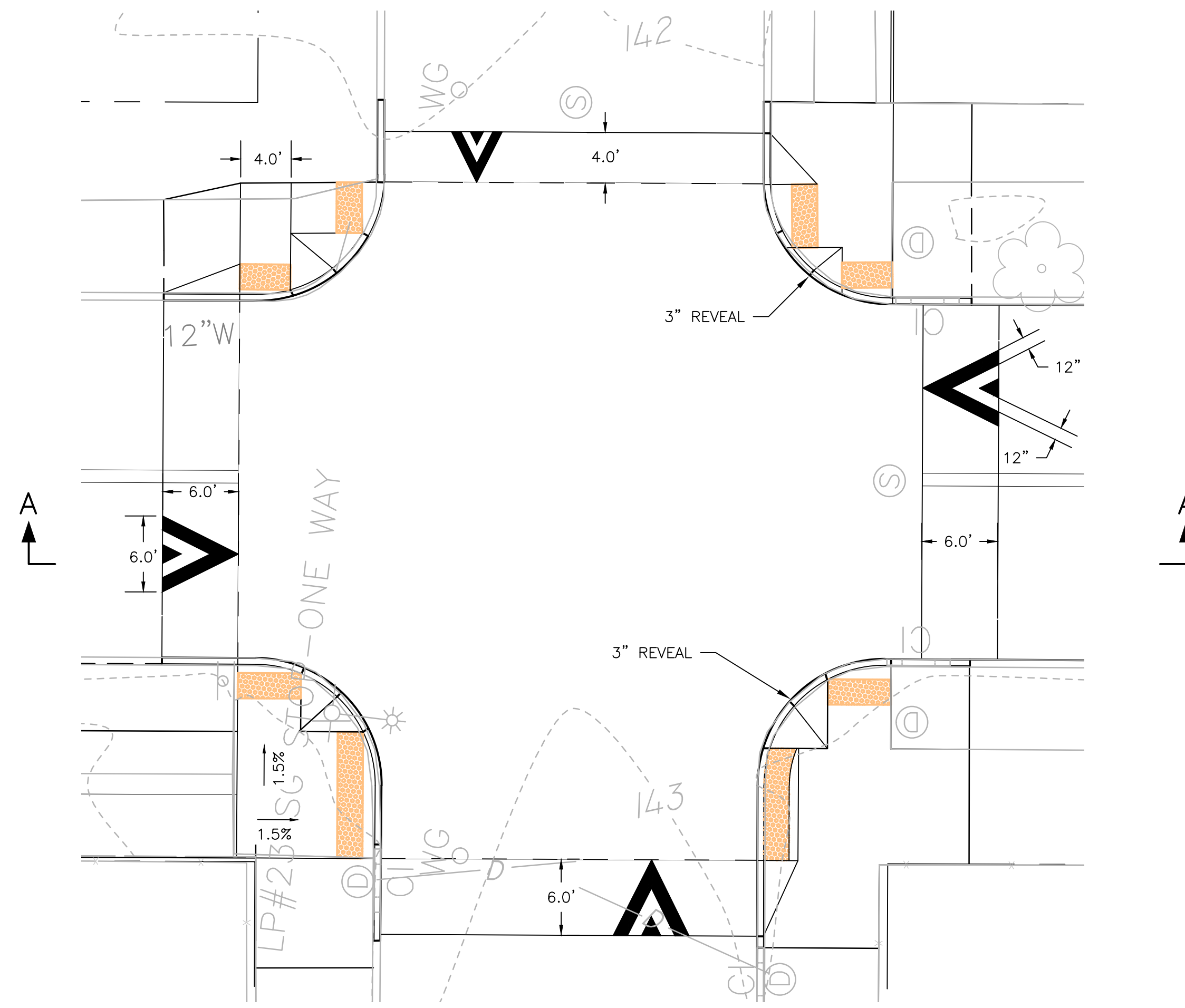
HIGHWAY SAFETY IMPROVEMENT PROGRAM
INTERSECTION SAFETY IMPROVEMENTS - STATEWIDE
PROVIDENCE, & JOHNSTON
JOB SPECIFIC LEGEND & NOTES



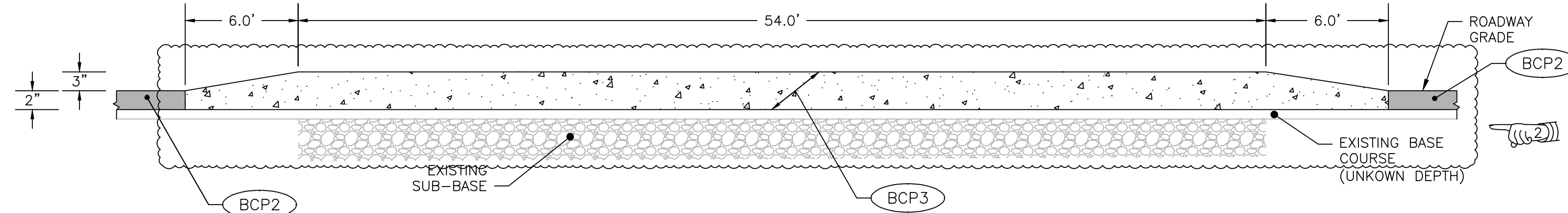
PLAN
SCALE: 1"=8'



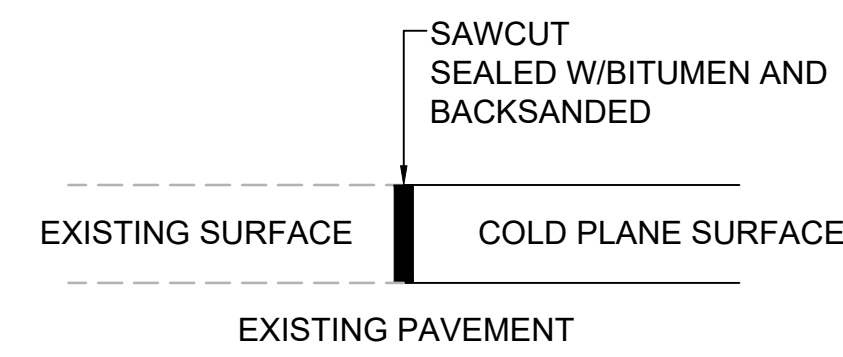
TYPICAL RAISED CROSSWALK DETAIL



PLAN
SCALE: 1"=8'



MOUNT PLEASANT AVENUE AT ROANOKE STREET RAISED INTERSECTION DETAIL



SAWCUT JOINT DETAIL @ LIMIT OF WORK
NOT TO SCALE

ADDENDUM NO. 3



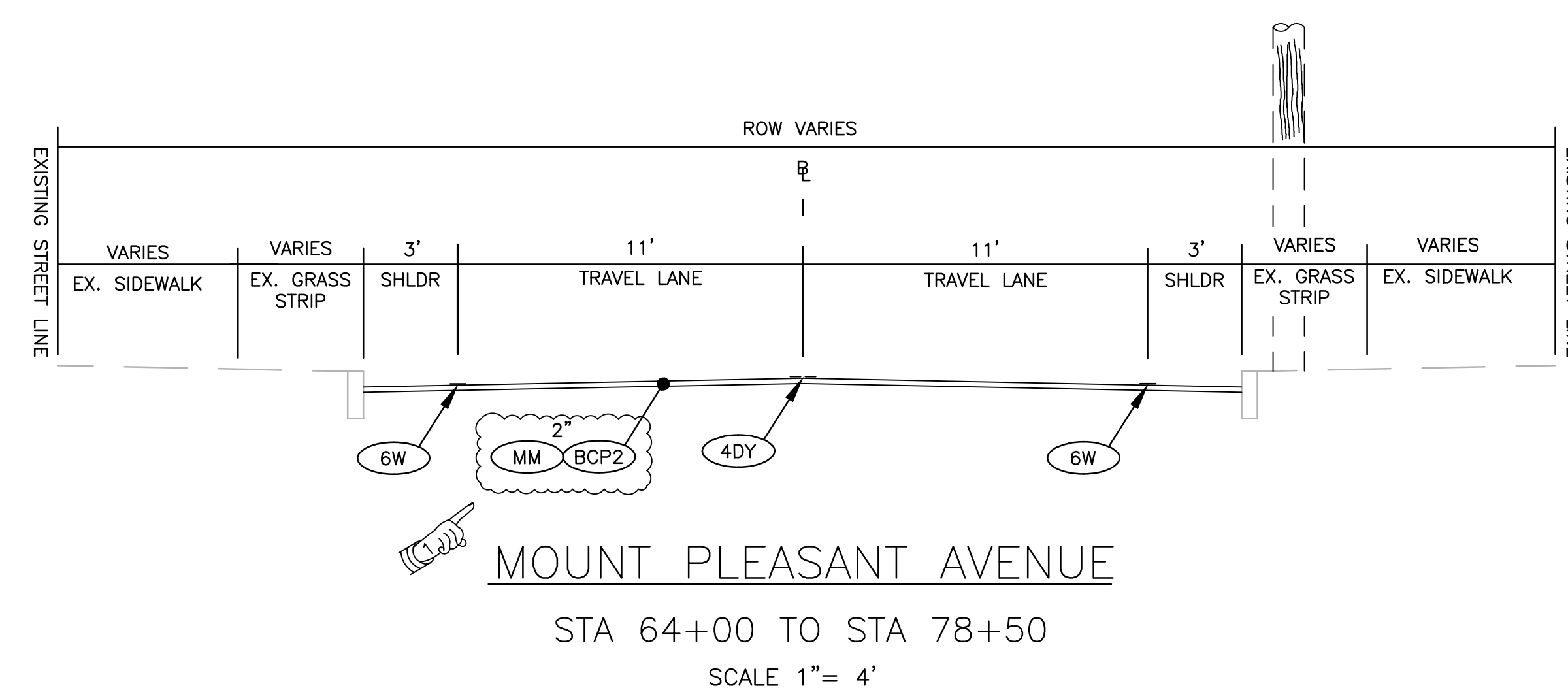
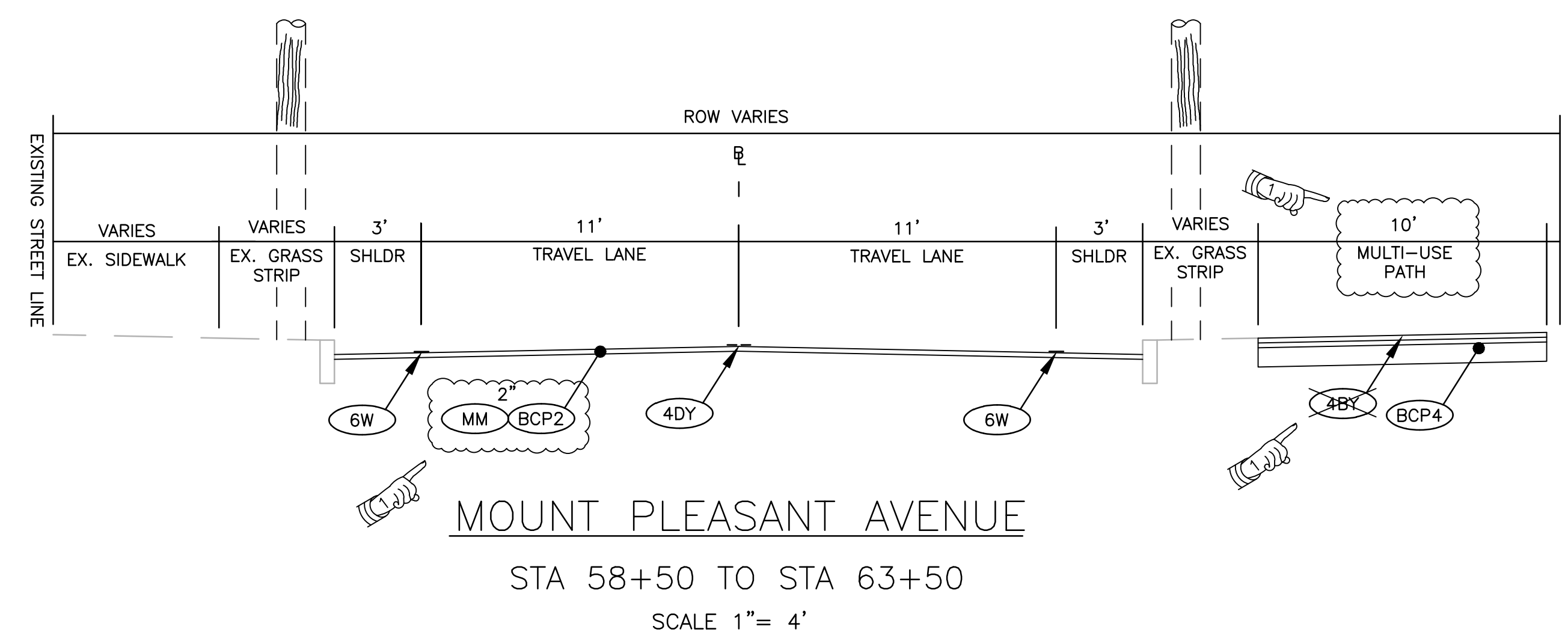
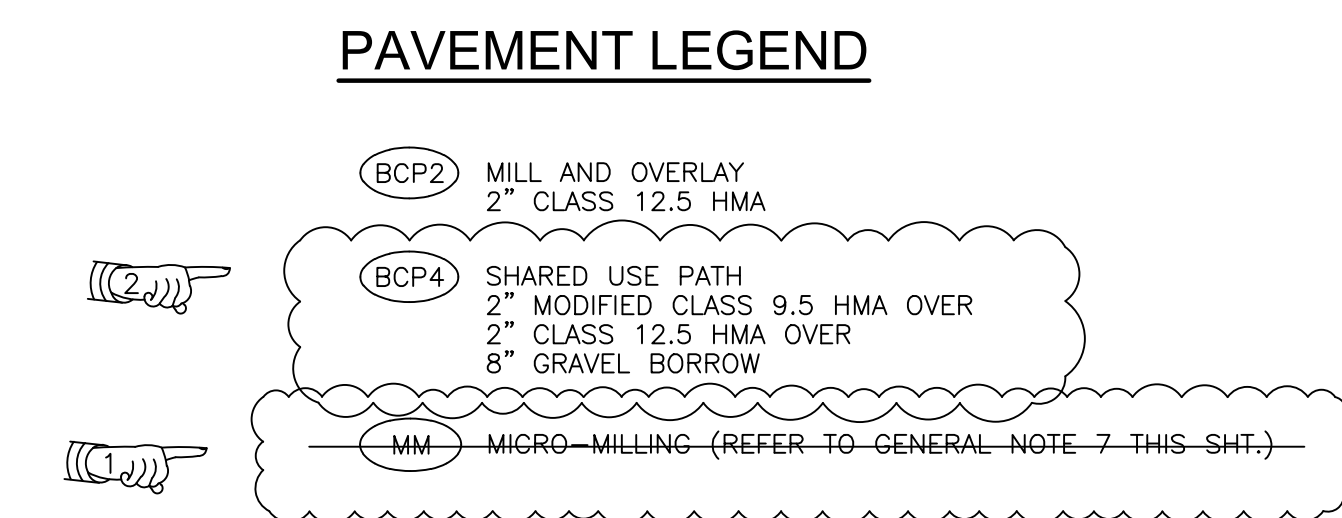
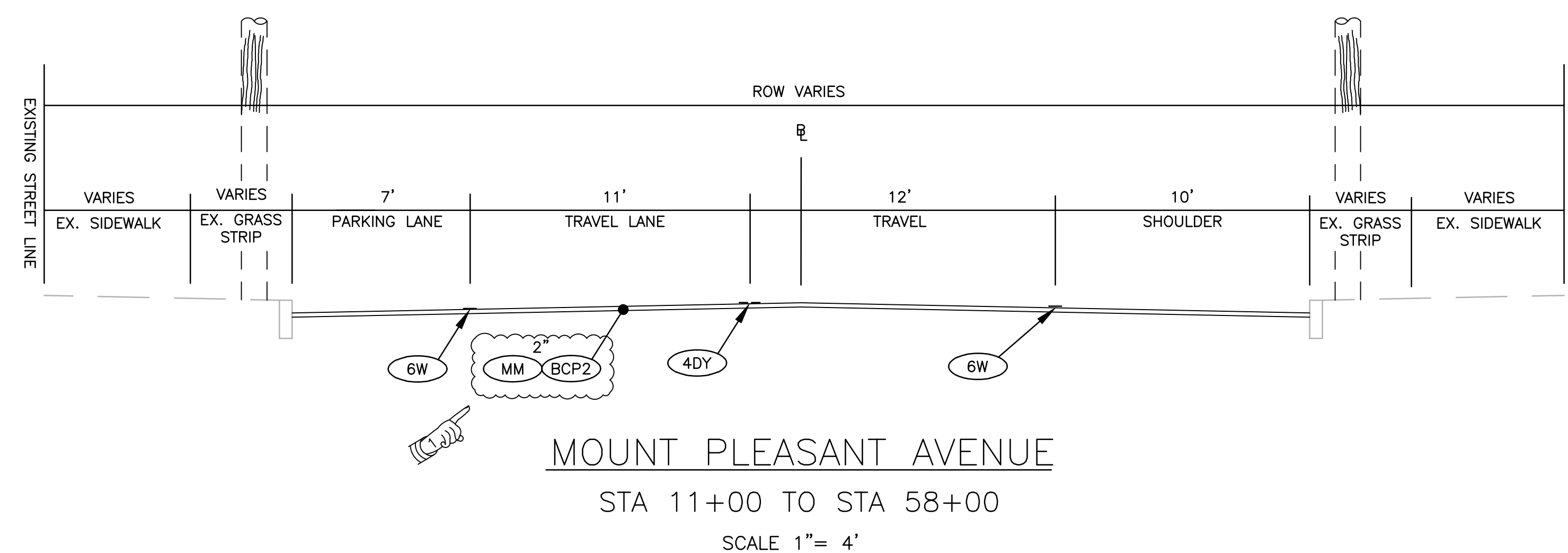
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE: SEPTEMBER 2021
SHEET: 9
OF: 77

AS SHOWN		REVISIONS			
NO.	DATE	BY	NO.	DATE	BY
1	10/6/21	BSL			
2	10/16/21	BSL			

HIGHWAY SAFETY IMPROVEMENT PROGRAM
INTERSECTION SAFETY IMPROVEMENTS - STATEWIDE
PROVIDENCE, & JOHNSTON
RHODE ISLAND

CONSTRUCTION DETAILS 2



ADDENDUM NO. 3



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE: SEPTEMBER 2021
SHEET: 12
OF: 77

NO SCALE		REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY		
1	10/6/21	BSL					
2	10/18/21	BSL					

HIGHWAY SAFETY IMPROVEMENT PROGRAM
INTERSECTION SAFETY IMPROVEMENTS - STATEWIDE
PROVIDENCE, & JOHNSTON
RHODE ISLAND

**MOUNT PLEASANT AVE -
TYPICAL SECTIONS**

"General Decision Number: RI20210001 10/15/2021

Superseded General Decision Number: RI20200001

State: Rhode Island

Construction Types: Building, Heavy (Heavy and Marine) and Highway

Counties: Rhode Island Statewide.

BUILDING CONSTRUCTION PROJECTS (does not include residential construction consisting of single family homes and apartments up to and including 4 stories) HEAVY, HIGHWAY AND MARINE CONSTRUCTION PROJECTS

Note: Under Executive Order (EO) 13658, an hourly minimum wage of \$10.95 for calendar year 2021 applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.95 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in calendar year 2021. If this contract is covered by the EO and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must pay workers in that classification at least the wage rate determined through the conformance process set forth in 29 CFR 5.5(a)(1)(ii) (or the EO minimum wage rate, if it is higher than the conformed wage rate). The EO minimum wage rate will be adjusted annually. Please note that this EO applies to the above-mentioned types of contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but it does not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(2)-(60). Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Modification Number	Publication Date
0	01/01/2021
1	01/22/2021
2	03/05/2021
3	04/09/2021
4	04/23/2021
5	06/18/2021
6	07/30/2021
7	09/10/2021
8	09/17/2021
9	10/15/2021

ASBE0006-006 12/01/2019

Rates Fringes

HAZARDOUS MATERIAL HANDLER
 (Includes preparation, wetting, stripping, removal scrapping, vacuuming, bagging & disposing of all insulation

materials, whether they contain asbestos or not, from mechanical systems).....\$ 36.60 22.40

ASBE0006-008 09/01/2021

Rates Fringes

Asbestos Worker/Insulator
Includes application of all insulating materials, protective coverings, coatings & finishes to all types of mechanical systems.\$ 45.00 32.89

BOIL0029-001 01/01/2021

Rates Fringes

BOILERMAKER.....\$ 45.87 29.02

BRRIO003-001 06/01/2020

Rates Fringes

Bricklayer, Stonemason, Pointer, Caulker & Cleaner.....\$ 42.55 28.02

BRRIO003-002 03/01/2020

Rates Fringes

Marble Setter, Terrazzo Worker & Tile Setter.....\$ 40.78 28.92

BRRIO003-003 03/01/2020

Rates Fringes

Marble, Tile & Terrazzo Finisher.....\$ 34.10 27.88

CARP0330-001 01/01/2021

Rates Fringes

CARPENTER (Includes Soft Floor Layer).....\$ 39.72 28.66
Diver Tender.....\$ 40.72 28.66
DIVER.....\$ 51.47 28.66
Piledriver.....\$ 39.72 28.66
WELDER.....\$ 40.72 28.66

FOOTNOTES:

When not diving or tending the diver, the diver and diver tender shall receive the piledriver rate. Diver tenders shall receive \$1.00 per hour above the pile driver rate when tending the diver.

Work on free-standing stacks, concrete silos & public utility electrical power houses, which are over 35 ft. in height when constructed: \$.50 per hour additional.

Work on exterior concrete shear wall gang forms, 45 ft. or

more above ground elevation or on setback: \$.50 per hour additional.

The designated piledriver, known as the ""monkey"": \$1.00 per hour additional.

CARP1121-002 01/06/2020

	Rates	Fringes
MILLWRIGHT.....	\$ 39.07	29.15

ELEC0099-002 06/02/2021

	Rates	Fringes
ELECTRICIAN.....	\$ 43.61	54.71%
Teledata System Installer.....	\$ 32.71	12.57%+14.93

FOOTNOTES:

Work of a hazardous nature, or where the work height is 30 ft. or more from the floor, except when working OSHA-approved lifts: 20% per hour additional.

Work in tunnels below ground level in combined sewer outfall: 20% per hour additional.

ELEV0039-001 01/01/2021

	Rates	Fringes
ELEVATOR MECHANIC.....	\$ 55.03	35.825+A+B

FOOTNOTES:

A. PAID HOLIDAYS: New Years Day; Memorial Day; Independence Day; Labor Day; Veterans' Day; Thanksgiving Day; the Friday after Thanksgiving Day; and Christmas Day.

B. Employer contributes 8% basic hourly rate for 5 years or more of service of 6% basic hourly rate for 6 months to 5 years of service as vacation pay credit.

ENGI0057-001 12/01/2020

	Rates	Fringes
Operating Engineer: (power plants, sewer treatment plants, pumping stations, tunnels, caissons, piers, docks, bridges, wind turbines, subterranean & other marine and heavy construction work)		
GROUP 1.....	\$ 42.55	27.70+a
GROUP 2.....	\$ 40.55	27.70+a
GROUP 3.....	\$ 36.17	27.70+a
GROUP 4.....	\$ 33.32	27.70+a
GROUP 5.....	\$ 39.60	27.70+a
GROUP 6.....	\$ 30.40	27.70+a

GROUP 7.....	\$ 24.40	27.70+a
GROUP 8.....	\$ 36.25	27.70+a
GROUP 9.....	\$ 40.17	27.70+a

a. BOOM LENGTHS, INCLUDING JIBS:

- 150 feet and over + \$ 2.00
- 180 feet and over + \$ 3.00
- 210 feet and over + \$ 4.00
- 240 feet and over + \$ 5.00
- 270 feet and over + \$ 7.00
- 300 feet and over + \$ 8.00
- 350 feet and over + \$ 9.00
- 400 feet and over + \$10.00

a. PAID HOLIDAYS:

New Year's Day, President's Day, Memorial Day, July Fourth, Victory Day, Labor Day, Columbus Day, Veterans Day, Thanksgiving Day, Christmas Day. a: Any employee who works 3 days in the week in which a holiday falls shall be paid for the holiday.

a. FOOTNOTES:

Hazmat work: \$2.00 per hour additional.
Tunnel/Shaft work: \$5.00 per hour additional.

POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1: Cranes, lighters, boom trucks and derricks

GROUP 2: Digging machine, Ross Carrier, locomotive, hoist, elevator, bidwell-type machine, shot & water blasting machine, paver, spreader, graders, front end loader (3 yds. and over), vibratory hammer & vacuum truck, roadheaders, forklifts, economobile type equipment, tunnel boring machines, concrete pump and on site concrete plants.

GROUP 3: Oilers on cranes.

GROUP 4: Oiler on crawler backhoe.

GROUP 5: Bulldozer, bobcats, skid steer loader, tractor, scraper, combination loader backhoe, roller, front end loader (less than 3 yds.), street and mobile-powered sweeper (3-yd. capacity), 8-ft. sweeper minimum 65 HP).

GROUP 6: Well-point installation crew.

GROUP 7: Utility Engineers and Signal Persons

GROUP 8: Heater, concrete mixer, stone crusher, welding machine, generator and light plant, gas and electric driven pump and air compressor.

GROUP 9: Boat & tug operator.

ENGI0057-002 11/01/2020

Rates Fringes

Power Equipment Operator
(highway construction
projects; water and sewerline

projects which are incidental to highway construction projects; and bridge projects that do not span water)

GROUP 1.....	\$ 35.70	27.70+a
GROUP 2.....	\$ 30.40	27.70+a
GROUP 3.....	\$ 24.40	27.70+a
GROUP 4.....	\$ 30.98	27.70+a
GROUP 5.....	\$ 34.68	27.70+a
GROUP 6.....	\$ 34.30	27.70+a
GROUP 7.....	\$ 29.95	27.70+a
GROUP 8.....	\$ 31.33	27.70+a
GROUP 9.....	\$ 33.28	27.70+a

a. FOOTNOTE: a. Any employee who works three days in the week in which a holiday falls shall be paid for the holiday.

a. PAID HOLIDAYS: New Year's Day, President's Day, Memorial Day, July Fourth, Victory Day, Labor Day, Columbus Day, Veterans Day, Thanksgiving Day & Christmas Day.

POWER EQUIPMENT OPERATOR CLASSIFICATIONS

GROUP 1: Digging machine, crane, piledriver, lighter, locomotive, derrick, hoist, boom truck, John Henry's, directional drilling machine, cold planer, reclaimer, paver, spreader, grader, front end loader (3 yds. and over), vacuum truck, test boring machine operator, veemere saw, water blaster, hydro-demolition robot, forklift, economobile, Ross Carrier, concrete pump operator and boats

GROUP 2: Well point installation crew

GROUP 3: Utlity engineers and signal persons

GROUP 4: Oiler on cranes

GROUP 5: Combination loader backhoe, front end loader (less than 3 yds.), forklift, bulldozers & scrapers and boats

GROUP 6: Roller,skid steer loaders, street sweeper

GROUP 7: Gas and electric drive heater, concrete mixer, light plant, welding machine, pump & compressor

GROUP 8: Stone crusher

GROUP 9: Mechanic & welder

ENGI0057-003 12/01/2020

BUILDING CONSTRUCTION

	Rates	Fringes
Power Equipment Operator		
GROUP 1.....	\$ 41.82	27.70+a
GROUP 2.....	\$ 39.82	27.70+a
GROUP 3.....	\$ 39.60	27.70+a
GROUP 4.....	\$ 35.60	27.70+a
GROUP 5.....	\$ 32.75	27.70+a
GROUP 6.....	\$ 38.90	27.70+a
GROUP 7.....	\$ 38.47	27.70+a
GROUP 8.....	\$ 35.79	27.70+a

a. BOOM LENGTHS, INCLUDING JIBS:

- 150 ft. and over: + \$ 2.00
- 180 ft. and over: + \$ 3.00
- 210 ft. and over: + \$ 4.00
- 240 ft. and over: + \$ 5.00
- 270 ft. and over: + \$ 7.00
- 300 ft. and over: + \$ 8.00
- 350 ft. and over: + \$ 9.00
- 400 ft. and over: + \$10.00

a. PAID HOLIDAYS: New Year's Day, President's Day, Memorial Day, July Fourth, Victory Day, Labor Day, Columbus Day, Veterans Day, Thanksgiving Day & Christmas Day. a: Any employee who works 3 days in the week in which a holiday falls shall be paid for the holiday.

a. FOOTNOTE: Hazmat work: \$2.00 per hour additional.
Tunnel/Shaft work: \$5.00 per hour additional.

POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1: Cranes, lighters, boom trucks and derricks.

GROUP 2: Digging machine, Ross carrier, locomotive, hoist, elevator, bidwell-type machine, shot & water blasting machine, paver, spreader, front end loader (3 yds. and over), vibratory hammer and vacuum truck

GROUP 3: Telehandler equipment, forklift, concrete pump & on-site concrete plant

GROUP 4: Fireman & oiler on cranes

GROUP 5: Oiler on crawler backhoe

GROUP 6: Bulldozer, skid steer loaders, bobcats, tractor, grader, scraper, combination loader backhoe, roller, front end loader (less than 3 yds.), street and mobile powered sweeper (3 yds. capacity), 8-ft. sweeper (minimum 65 hp)

GROUP 7: Well point installation crew

GROUP 8: Heater, concrete mixer, stone crusher, welding machine, generator for light plant, gas and electric driven pump & air compressor

IRON0037-001 09/16/2021

	Rates	Fringes
IRONWORKER.....	\$ 38.21	30.58

LABO0271-001 05/30/2021

BUILDING CONSTRUCTION

	Rates	Fringes
LABORER		
GROUP 1.....	\$ 33.55	26.15
GROUP 2.....	\$ 33.80	26.15

GROUP 3.....	\$ 34.30	26.15
GROUP 4.....	\$ 34.55	26.15
GROUP 5.....	\$ 35.55	26.15

LABORERS CLASSIFICATIONS

GROUP 1: Laborer, Carpenter Tender, Mason Tender, Cement Finisher Tender, Scaffold Erector, Wrecking Laborer, Asbestos Removal [Non-Mechanical Systems]

GROUP 2: Asphalt Raker, Adzemen, Pipe Trench Bracer, Demolition Burner, Chain Saw Operator, Fence & Guard Rail Erector, Setter of Metal Forms for Roadways, Mortar Mixer, Pipelayer, Riprap & Dry Stonewall Builder, Highway Stone Spreader, Pneumatic Tool Operator, Wagon Drill Operator, Tree Trimmer, Barco-Type Jumping Tamper, Mechanical Grinder Operator

GROUP 3: Pre-Cast Floor & Roof Plank Erectors

GROUP 4: Air Track Operator, Hydraulic & Similar Self-Powered Drill, Block Paver, Rammer, Curb Setter, Powderman & Blaster

GROUP 5: Toxic Waste Remover

LAB00271-002 05/30/2021

HEAVY AND HIGHWAY CONSTRUCTION

	Rates	Fringes
LABORER		
COMPRESSED AIR		
Group 1.....	\$ 53.45	24.15
Group 2.....	\$ 50.98	24.15
Group 3.....	\$ 40.50	24.15
FREE AIR		
Group 1.....	\$ 44.05	24.15
Group 2.....	\$ 43.05	24.15
Group 3.....	\$ 40.50	24.15
LABORER		
Group 1.....	\$ 33.55	24.15
Group 2.....	\$ 33.80	24.15
Group 3.....	\$ 34.55	24.15
Group 4.....	\$ 27.05	24.15
Group 5.....	\$ 35.55	24.15
OPEN AIR CAISSON, UNDERPINNING WORK AND BORING CREW		
Bottom Man.....	\$ 39.55	24.15
Top Man & Laborer.....	\$ 38.60	24.15
TEST BORING		
Driller.....	\$ 40.00	24.15
Laborer.....	\$ 38.60	24.15

LABORER CLASSIFICATIONS

GROUP 1: Laborer; Carpenter tender; Cement finisher tender; Wrecking laborer; Asbestos removers [non-mechanical systems]; Plant laborer; Driller in quarries

GROUP 2: Adzeperson; Asphalt raker; Barcotype jumping tamper; Chain saw operators; Concrete and power buggy operator; Concrete saw operator; Demolition burner; Fence

and guard rail erector; Highway stone spreader; Laser beam operator; Mechanical grinder operator; Mason tender; Mortar mixer; Pneumatic tool operator; Riprap and dry stonewall builder; Scaffold erector; Setter of metal forms for roadways; Wagon drill operator; Wood chipper operator; Pipelayer; Pipe trench bracer

GROUP 3: Air track drill operator; Hydraulic and similar powered drills; Brick paver; Block paver; Rammer and curb setter; Powderperson and blaster

GROUP 4: Flagger & signaler

GROUP 5: Toxic waste remover

LABORER - COMPRESSED AIR CLASSIFICATIONS

GROUP 1: Mucking machine operator, tunnel laborer, brake person, track person, miner, grout person, lock tender, gauge tender, miner: motor person & all others in compressed air

GROUP 2: Change house attendant, powder watchperson, top person on iron

GROUP 3: Hazardous waste work within the "HOT" zone

LABORER - FREE AIR CLASSIFICATIONS

GROUP 1: Grout person - pumps, brake person, track person, form mover & stripper (wood & steel), shaft laborer, laborer topside, outside motorperson, miner, conveyor operator, miner welder, heading motorperson, erecting operator, mucking machine operator, nozzle person, rodperson, safety miner, shaft & tunnel, steel & rodperson, mole nipper, concrete worker, form erector (wood, steel and all accessories), cement finisher (this type of work only), top signal person, bottom person (when heading is 50' from shaft), burner, shield operator and TBM operator

GROUP 2: Change house attendant, powder watchperson

GROUP 3: Hazardous waste work within the "HOT" zone

PAIN0011-005 06/01/2021

	Rates	Fringes
PAINTER		
Brush and Roller.....	\$ 36.42	22.90
Epoxy, Tanks, Towers, Swing Stage & Structural Steel.....	\$ 38.42	22.90
Spray, Sand & Water Blasting.....	\$ 39.42	22.90
Taper.....	\$ 37.17	22.90
Wall Coverer.....	\$ 36.92	22.90

PAIN0011-006 06/01/2021

	Rates	Fringes
GLAZIER.....	\$ 39.98	22.90

FOOTNOTES:

SWING STAGE: \$1.00 per hour additional.

PAID HOLIDAYS: Labor Day & Christmas Day.

PAIN0011-011 06/01/2021

	Rates	Fringes
Painter (Bridge Work).....	\$ 54.00	22.90

PAIN0035-008 06/01/2011

	Rates	Fringes
Sign Painter.....	\$ 24.79	13.72

PLAS0040-001 06/03/2019

BUILDING CONSTRUCTION

	Rates	Fringes
CEMENT MASON/CONCRETE FINISHER...	\$ 36.00	27.15

FOOTNOTE: Cement Mason: Work on free swinging scaffolds under 3 planks width and which is 20 or more feet above ground and any offset structure: \$.30 per hour additional.

PLAS0040-002 07/01/2019

HEAVY AND HIGHWAY CONSTRUCTION

	Rates	Fringes
CEMENT MASON/CONCRETE FINISHER...	\$ 32.85	22.20

PLAS0040-003 07/01/2019

	Rates	Fringes
PLASTERER.....	\$ 37.55	27.50

PLUM0051-002 08/30/2021

	Rates	Fringes
Plumbers and Pipefitters.....	\$ 46.49	31.40

ROOF0033-004 06/01/2021

	Rates	Fringes
ROOFER.....	\$ 39.40	29.06

SFRI0669-001 04/01/2021

	Rates	Fringes
SPRINKLER FITTER.....	\$ 47.55	26.60

SHEE0017-002 12/01/2020

	Rates	Fringes
Sheet Metal Worker.....	\$ 38.58	36.73

TEAM0251-001 05/01/2019

HEAVY AND HIGHWAY CONSTRUCTION

	Rates	Fringes
TRUCK DRIVER		
GROUP 1.....	\$ 27.96	26.8525+A+B+C
GROUP 2.....	\$ 27.61	26.8525+A+B+C
GROUP 3.....	\$ 27.66	26.8525+A+B+C
GROUP 4.....	\$ 27.71	26.8525+A+B+C
GROUP 5.....	\$ 27.81	26.8525+A+B+C
GROUP 6.....	\$ 28.21	26.8525+A+B+C
GROUP 7.....	\$ 28.41	26.8525+A+B+C
GROUP 8.....	\$ 27.91	26.8525+A+B+C
GROUP 9.....	\$ 28.16	26.8525+A+B+C
GROUP 10.....	\$ 27.96	26.8525+A+B+C

FOOTNOTES:

A. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas Day, plus Presidents' Day, Columbus Day, Veteran's Day & V-J Day, providing the employee has worked at least one day in the calendar week in which the holiday falls.

B. Employee who has been on the payroll for 1 year or more but less than 5 years and has worked 150 Days during the last year of employment shall receive 1 week's paid vacation; 5 to 10 years - 2 weeks' paid vacation; 10 or more years - 3 week's paid vacation.

C. Employees on the seniority list shall be paid a one hundred dollar (\$100.00) bonus for every four hundred (400) hours worked, up to a maximum of five hundred dollars (\$500.00)

All drivers working on a defined hazard material job site shall be paid a premium of \$2.00 per hour over applicable rate.

TRUCK DRIVER CLASSIFICATIONS

GROUP 1: Pick-up trucks, station wagons, & panel trucks

GROUP 2: Two-axle on low beds

GROUP 3: Two-axle dump truck

GROUP 4: Three-axle dump truck

GROUP 5: Four- and five-axle equipment

GROUP 6: Low-bed or boom trailer.

GROUP 7: Trailers when used on a double hook up (pulling 2 trailers)

GROUP 8: Special earth-moving equipment, under 35 tons

GROUP 9: Special earth-moving equipment, 35 tons or over

GROUP 10: Tractor trailer

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

=====
Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of "identifiers" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than "SU" or "UAVG" denotes that the union classification and rate were prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate

changes in the collective bargaining agreement (CBA) governing this classification and rate.

Survey Rate Identifiers

Classifications listed under the ""SU"" identifier indicate that no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the

Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

=====

END OF GENERAL DECISION"